



TM

***Altamont***  
***MOTORSPORTS PARK***

ALAMEDA COUNTY PLANNING DEPARTMENT

APPLICATION FOR REZONING

Submitted: 18 May 2007

Rev. 1.5

## **CONTENTS**

<b>Project Description</b>	<b>3</b>
<b>Project Components</b>	<b>3</b>
<b>Competition Venue</b>	<b>3</b>
<b>Primary Track</b>	<b>3</b>
<b>General Location</b>	<b>4</b>
<b>Current (2007) Site Plan</b>	<b>5</b>
<b>Pit &amp; Paddock Area</b>	<b>7</b>
<b>Buffer Area</b>	<b>7</b>
<b>Permitted Uses</b>	<b>7</b>
<b>The Racing Season: Spectator Events</b>	<b>9</b>
<b>The Racing Season: Non-Spectator Events</b>	<b>10</b>
<b>Hours of Operation / Weekly Schedule</b>	<b>10</b>
<b>Capacity Expectations</b>	<b>11</b>
<b>Grandstand &amp; Guest Services</b>	<b>12</b>
<b>Race Operations and Facility Support Shop</b>	<b>13</b>
<b>Guest Parking &amp; Buffer</b>	<b>13</b>
<b>Operational Parameters &amp; Restrictions</b>	<b>15</b>
<b>Recreational Vehicles</b>	<b>16</b>
<b>Residential Uses</b>	<b>16</b>
<b>Acoustical Standards</b>	<b>17</b>
<b>Signage</b>	<b>17</b>
<b>Lighting</b>	<b>21</b>
<b>Utilities</b>	<b>22</b>
<b>Landscaping</b>	<b>24</b>
<b>Fencing</b>	<b>24</b>
<b>Paving and Impervious Surfaces</b>	<b>25</b>
<b>Public Services</b>	<b>25</b>
<b>Helicopters</b>	<b>26</b>
<b>Safety and Fire Suppression</b>	<b>26</b>
<b>Traffic &amp; Circulation</b>	<b>26</b>
<b>Storage</b>	<b>27</b>
<b>Refuse, Trash &amp; Debris</b>	<b>27</b>
<b>EXHIBITS</b>	<b>A - G</b>

## PROJECT DESCRIPTION

The proposed rezoning of the Altamont Motorsports Park (AMP) from “A - General Agriculture” to a “PD - Planned Development” district will provide for the continued operation of a multi-venue, motorsports-based outdoor recreation and entertainment facility in Alameda County, California. AMP opened in 1963 as a dirt oval raceway, being paved and re-configured in 1966 to feature three (3) separate race tracks integrated within the perimeter of the larger ½-mile oval track. The raceway has been and is envisioned to continue as a “regional” motorsports facility, approved and sanctioned to conduct amateur, semi-professional and professional motorsports testing and events. As a regional facility, motorsports testing and events are expected to draw competitors from a 200-mile radius. Spectators are expected to be attracted from a 100-mile radius for professional and touring shows, and a 50-mile radius for amateur and/or semi-professional events.

The AMP site consists of approximately 83 acres located approximately 10 miles east of the City of Livermore (approximately seven miles west of Tracy) in the eastern portion of Alameda County. The facility is located immediately south of the I-580 / I-205 interchange. Irregularly shaped, the site is generally bounded by I-580 to the north and I-205 to the east (see Figure 1, next page). To the south is a private residence, built in 2004-2005 and occupied in December 2005, with a commercial, 30-acre motorcycle motocross track just east of the hilltop residence. To the west of the site, on Midway Road, are approximately 10 rural, agricultural residences.

Topographically, the site consists of rolling hills. Elevation variations exceed 100’ from the highest to the lowest locations on the property. A narrow band near the northeast corner of the site, the lowest elevation of the property, contains random trees and grasses, irrigated by seasonal rainfall which collects within an on-site detention pond.

Of the 83-acre site, approximately 35 acres are developed and contain the actual racetracks, pit/paddock parking area, grandstands and other supporting infrastructure. The remainder of the property, approximately 50 acres, is outside of the developed activity area and is retained in an undeveloped condition that is vegetated with natural grasses. The areas of natural grasses are devoid of other means of irrigation beyond seasonal rain.

The site also contains easements for the State of California Department of Transportation (Caltrans) adjoining the I-580/I-205 freeways, as well as a 40-foot Public Utilities Easement (PUE) along the southern boundary of the property which further serves as an accessway for a smaller, private 20-acre site to the east of the AMP property.

## PROJECT COMPONENTS

1. **Competition Venue** - A single motorsports venue exists and has been in continuous operation for over 43 years. This venue is referred to as the “Primary Track.” A detailed description includes:

**A) Primary Track – The Primary Track includes** a one-half mile, banked oval racetrack (“Speedway”) and an area extending fifty (50) feet from the outermost paved section of the racetrack (see Figure 2). The Speedway is and has been used for NASCAR, USAC, and other motorsports vehicles for testing and events, and is the largest track (by area); when used solely as a paved oval, the Speedway is used primarily by stock cars, trucks and open-wheel vehicles (sprint cars and/or formula cars).

The developed area including, surrounding and located within the larger, ½-mile paved oval raceway is includes an area extending fifty (50) feet from the outermost paved section of the racetrack (see Figure 2), which provides for associated safety devices, safety catch fencing, racetrack lighting systems, public address and information systems, imbedded electronic systems, race team spotting stations, media access areas, track utilities, accessways and other items related to the operation and/or maintenance of the Primary Track. As previously stated, the Primary Track, including this ½ -mile Speedway currently exists.

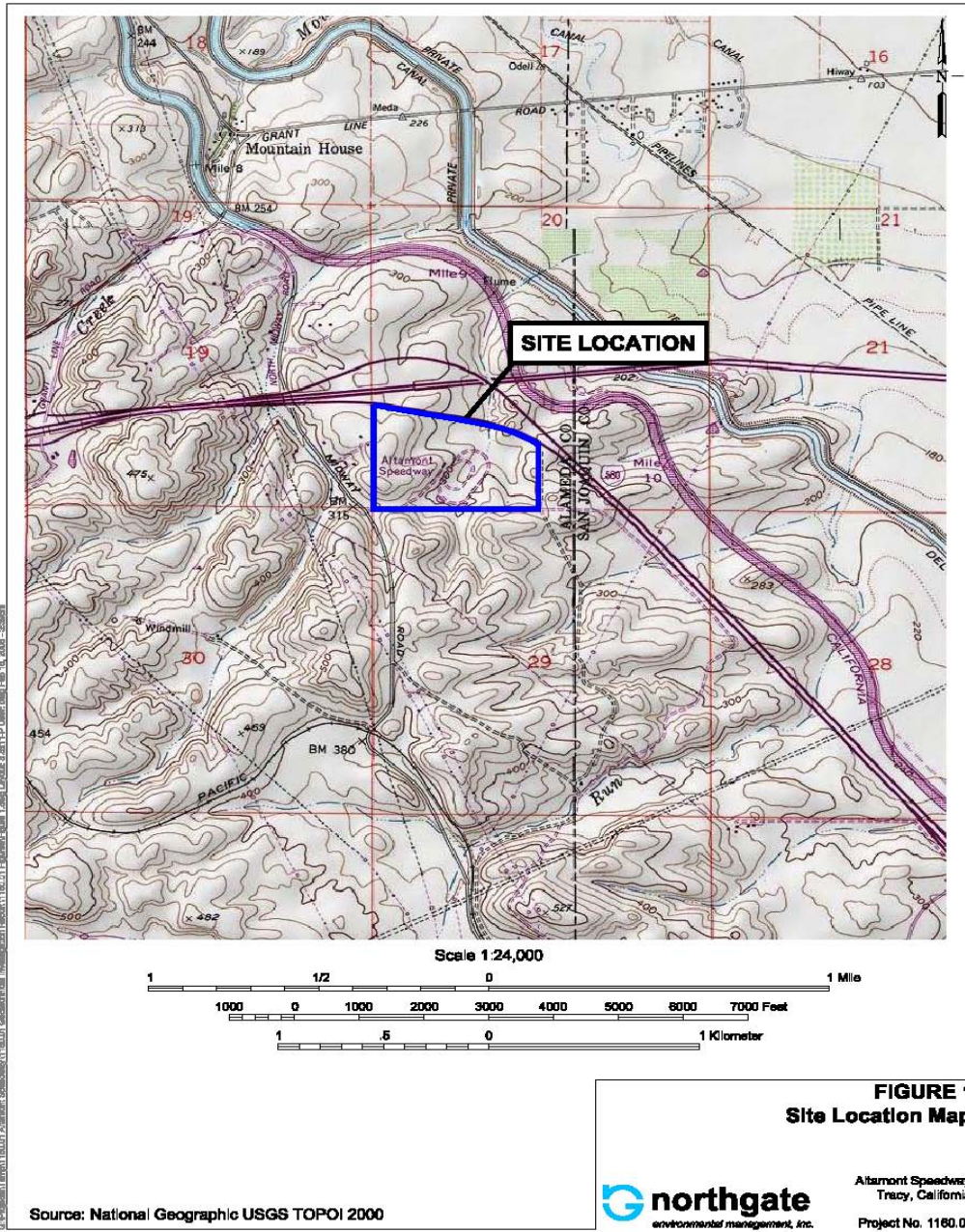


Figure 1 – General Location and Topography

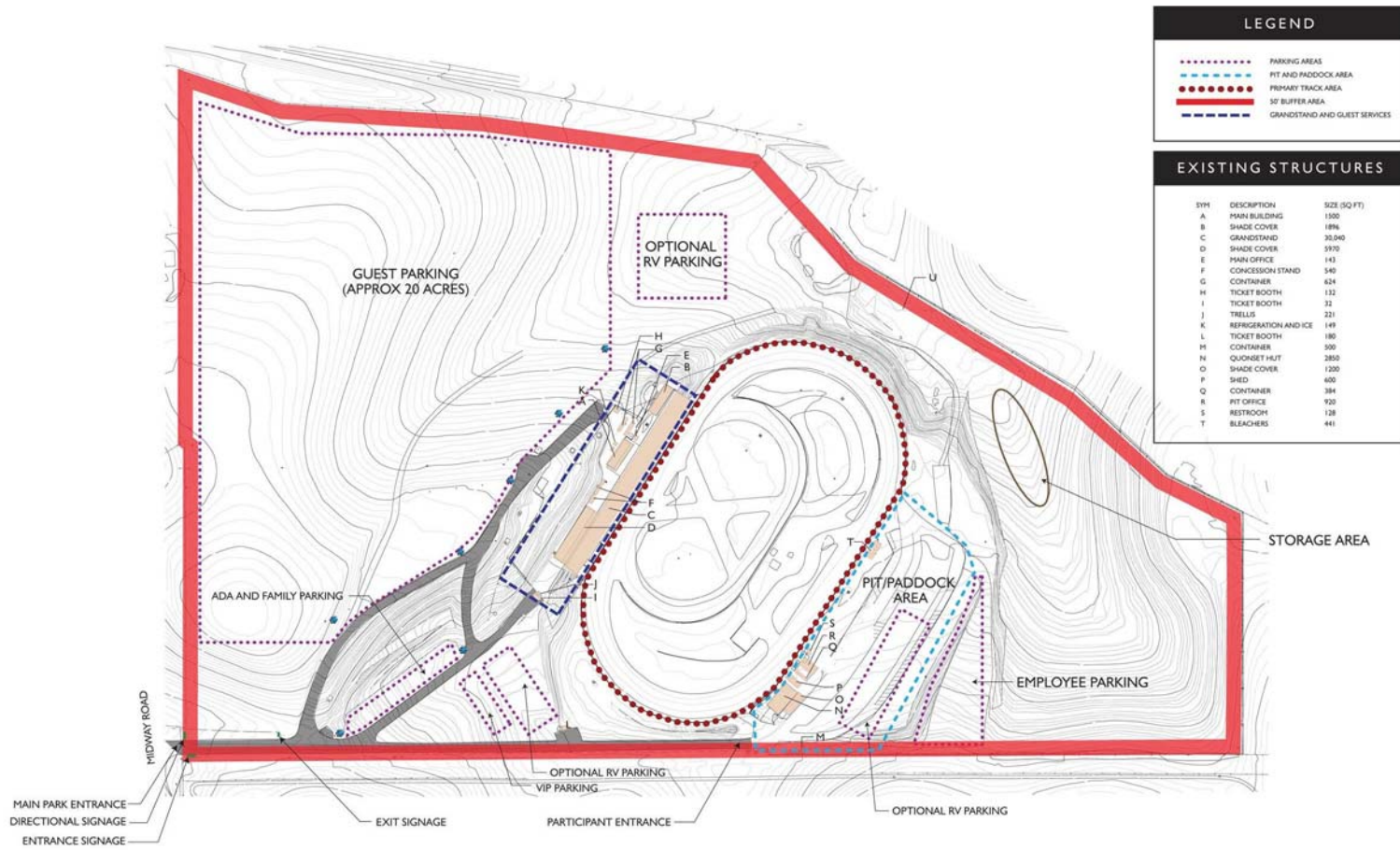
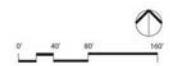


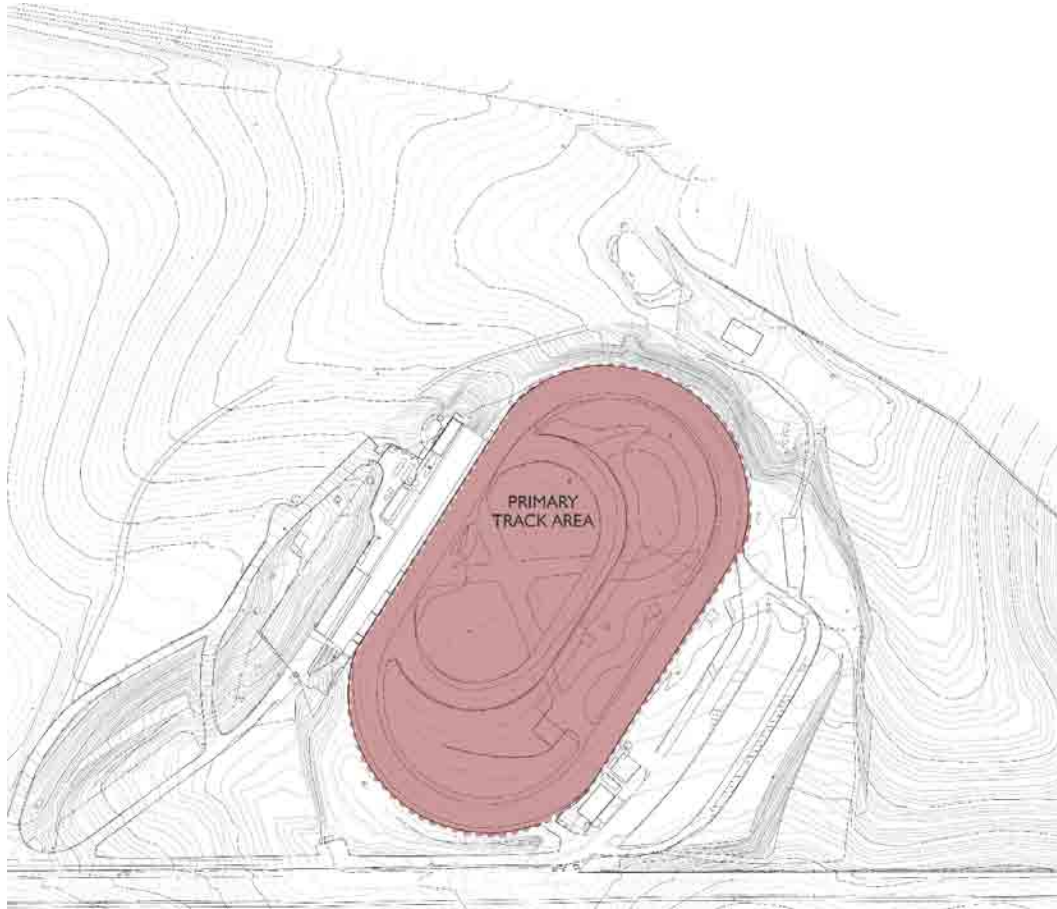
Figure 1a – Existing Site Plan

**GATES & ASSOCIATES**  
 LANDSCAPE ARCHITECTURE  
 LAND PLANNING URBAN DESIGN  
 8871 DREW GANTON RD. SAN RAMON, CA 94583  
 TEL 925.706.6176 FAX 925.452.8001  
 WWW.GATES.COM

**ALTAMONT MOTORSPORTS PARK**  
 TRACY, CA  
 MAY 8, 2007

EXISTING SITE PLAN





**Figure 2 – Primary Track Area**

Further integrated within the Primary Track area are three (3) additional track layouts:

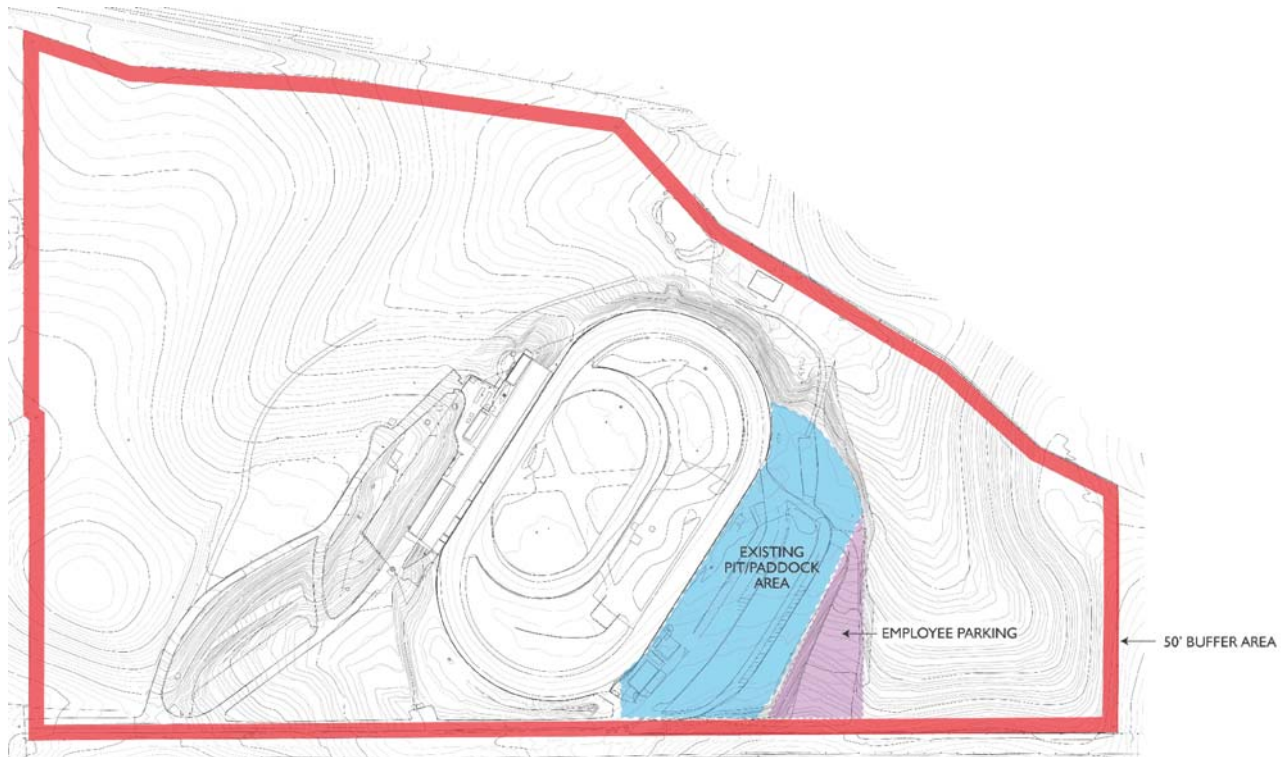
- i. **One-quarter Mile Oval** – Integrated within the Primary Track is a  $\frac{1}{4}$ -mile Paved Oval, similar in nature to the  $\frac{1}{2}$ -mile oval, for use by mini-truck, stockcar, and smaller open-wheel racing vehicles for testing and events; however, speeds are significantly reduced, making this venue more applicable to vehicles with smaller engines and/or lower horsepower. This track currently exists.
- ii. **Figure-8 Track** – Utilizing a combination of the  $\frac{1}{4}$ -mile oval and a specially integrated cross-pattern or “X” track design; Figure-8 racing features stockcars, small sedans and trucks. This track currently exists.
- iii. **Road Course** – Stockcar, autocross, enduro, kart, motorcycle, drifting, formula-sports car, and open-wheel racing vehicles utilize this 1.1-mile, variable configuration track with both left- and right-hand turns. This course currently exists and incorporates portions of the  $\frac{1}{2}$ - and  $\frac{1}{4}$ -mile paved oval tracks and Figure-8 track.

2. **Pit & Paddock Area** – Located to the east of the Primary Track, this area has been, continues to and will be used primarily as a parking area for racecar team transporters, team recreational vehicles, team hospitality vehicles and as a working area for race teams on their racecars, motorcycles, karts and/or other competition vehicles during events held within the Primary Track (see Figure 3). To the east of the Pit and Paddock Area is employee parking, located on a raised, flattened berm of fill dirt imported to the site in the late 1990s.

Pit and Paddock-based services have and will provide all the required amenities for guests and competitors including restroom facilities and concessions.

The Pit and Paddock area is approximately 9 acres in area. Employee parking is approximately 3 acres.

3. **Buffer Area** – A 50' buffer area exists around all property boundaries.



**Figure 3 – Pit & Paddock Area**

4. **Permitted Uses:**

- A) Competition Motorsports Events – Any current, conventional or historically sanctioned form of motorsport event is allowed within the Primary Track, pursuant to all other restrictions and conditions identified herein, with the exception of drag racing and/or mud-bog racing. The following definitions shall be maintained:

- i. A “sanctioned” form of motorsports is defined as any type, class, or style of competition or motorsports event or automotive/motorcycle exhibition where an entity (corporation, LLC, organization, club, DBA, etc.) has organized the

competition or event or exhibition and the entity can provide proof of any two (2) of the following:

a) The entity is an established motorsports promoter and/or sanctioning body which licenses its own competitors and maintains its own competition and/or technical rules and regulations pertaining to its events; or

b) The entity maintains liability insurance under its name in the amount of \$1 million or greater per occurrence; or

c) The entity maintains a registered membership of greater than 50 members; or

d) The entity has managed and conducted at least two (2) prior competitions or events or exhibitions within the last 24 months; these competitions or events or exhibitions held at a valid and recognized motorsports facility.

- ii. Any and all bicycle events (BMX or other application) or non-motorized competition or private event uses are exempt from sanctioning requirements and such events may be held under the direction of any organization, including AMP.
- iii. As an exception to the permitted uses, Drag Racing is defined as a condition where two (2) similarly matched vehicles, based on power-to-weight ratios, compete in a side-by-side, straight-line race to obtain the fastest time within a measured distance and where the winner of the competition obtains a faster time than that of the adjacent competing vehicle.
- iv. As an exception to the permitted uses, Mud-bog Racing is defined as any event where a vehicle is purposefully engaged in any competing event that involves a fixed course that is comprised entirely or of the majority of mud or similar water/dirt slurry solution.

**B) Non-Competition Events:** The following Non-Competition Events will be incidental to the primary Competition Motorsports Events at AMP and included as permitted uses:

- i. Live Music (concerts) and/or recorded music, during and within 24 hours of any scheduled Competition Motorsports Event and/or Automotive-Related Event;
- ii. Automotive-Related Events, which are defined as motorcycle/automotive shows, motorcycle/automotive-related shows, motorcycle/automotive and/or motorsports swap meets, and/or other motorcycle/automotive events or exhibitions within the Primary Track and/or Pit and Paddock areas.
- iii. Competition Vehicle Testing, which is defined as a driving or driver practice, driving or driver evaluation, vehicle performance test, speed test, component test, vehicle or system evaluation, and/or other engineering tests are being performed utilizing a competition vehicle; and, no direct competitive contest with another vehicle is being engaged. Competition Vehicle Testing is a non-spectator event.
- iv. Non-competition Vehicle Testing, which is defined as a driving or driver practice, driving or driver evaluation, vehicle performance test, speed test, component test, vehicle or system evaluation, and/or other engineering tests are being performed utilizing a non-competition vehicle; and, no

- direct competitive contest with another vehicle is being engaged. Non-competition Vehicle Testing is a non-spectator event.
- v. Private Events and Private Use, which are defined as private competition events or non-competition vehicle tests or exhibitions, television and film use, and/or private motorsports and/or automotive-related events by any entity, club or organization that may not adhere to a fixed racing season or schedule.

Concerts are anticipated to include events that would be held within one day (+/- 24 hour period) of a scheduled motorsports event, as well as events held in conjunction with (on the same day) a motorsports event (e.g. – car show, swap meet, etc.). A concert would only be allowed Friday through Sunday, and on National holidays. Concerts shall be staged such that all amplified sound and speaker systems shall be oriented towards either the grandstands or northeastern portions of the property, and away from all existing residential development near or adjacent to the facility (generally identified as being in the southern and south-western direction of the property when viewed from the center of the Primary Track).

Depending on the type of musical event and/or supporting motorsports event scheduled to take place, it could occur such that either the music source or the audience is located in the grandstands, within the infield, or within the Pit and Paddock area. Other restrictions pertaining to concerts would include:

- o Days/times allowed for concerts:
  - Fridays – 6:00 p.m. through 11:00 p.m.
  - Saturdays, Sundays, National Holidays – 11:00 a.m. through 11:00 p.m.
- o Live concerts to utilize event-specific amplification and speaker system
- o Public address announcements and recorded music to utilize resident and fixed AMP sound system.
- o Concerts types to include all contemporary types of music, including but not limited to:
  - Classical
  - Jazz / R&B
  - Rock / Hip-hop / Rap
  - Country / Western / Folk / Bluegrass

**5. The Racing Season: Competition and Spectator-approved Events** - AMP will hold all spectator-based competition motorsports events between 1 March and 30 November of any given year. The annual season opening event may or may not occur on 1 March; likewise, the annual season closing event may or may not occur on 30 November. The types of events occurring during that season include non-feature, feature and major feature events.

**Non-Feature Events** will typically occur 20-25 weekends per year. Such events will primarily attract spectators within a 35-mile radius, with generally up to 4,000 guests per event (primarily Saturday evening), depending upon the mix and schedule of the event.

**Feature Events** will typically occur 10-15 weekends per year and are in addition to the Non-feature weekends. Feature events will attract spectators from up to 100 miles away, with attendance generally ranging from 4,000 to 6,000 guests.

**Major-Feature Events** will typically occur 5-10 weekends per year and are in addition to the Non-feature and Feature weekends. Major-Feature events will attract spectators from up to 200 miles away, with attendance generally ranging from 6,000 to the maximum allowable 8,000 guests.

- 6. The Racing Season: Non-Competition and Non-Spectator Approved Events** - In addition to weekend competition motorsports events, AMP will continue to feature mid-week practice sessions, private events, private use, private practice and semi-private track rental programs, as well as advanced high-performance driving schools, competition driving schools, and media and commercial filming, as these activities have been on-going at the site for over 40 years.

While AMP's annual, spectator-based event operating period (1 March through 30 November) defines the spectator-based racing season, some motorsports entities do not adhere to any "typical" racing season, as defined by AMP or other motorsports organizations; therefore, other Non-Competition Events and/or Non-Spectator Events may be held year round within the Pit and Paddock area and/or within the confines of the Primary Track, depending on availability of any specific venue, area or track. During the off-season, it is estimated that the park may draw between 5-500 individuals associated with any specific non-spectator event per weekend day. During any Non-Spectator Event, all individuals on-site must be affiliated with a competitor, competition team, sponsor, the promoter or by some other directly associated means to the event.

- 7. Hours of Operation and Typical Weekly Schedule of Operations** – The hours of operation shall be:
- Monday – 9:00am – 5:00pm
  - Tuesday – 9:00am – 5:00pm
  - Wednesday – 9:00am – 10:00pm (with extended curfew, if required, to 11:00pm)
  - Thursday – 9:00am – 10:00pm (with extended curfew, if required, to 11:00pm)
  - Friday – 9:00am – 10:00pm (with extended curfew, if required, to 11:00pm)
  - Saturday – 9:00am – 10:00pm (with extended curfew, if required, to 11:00pm)
  - Sunday – 9:00am – 10:00pm (with extended curfew, if required, to 11:00pm)

The following is an anticipated standard weekly schedule of annual operations for AMP. It is provided to illustrate the more common operations of the facility, and is subject to adjustment:

**Monday:**

- *Daytime:* AMP-affiliated driving schools; private, semi-private and team track rentals (9:00am – 5:00pm)
- *Evening:* No activities or events.

**Tuesday:**

- *Daytime:* AMP-affiliated driving schools; private, semi-private and team track rentals (9:00am – 5:00pm)
- *Evening:* No activities or events.

**Wednesday:**

- *Daytime:* AMP-affiliated driving schools; private, semi-private and team track rentals (9:00am – 2:00pm)
- *Evening:* Competition vehicle testing (2:00pm – 9:00pm).

**Thursday:**

- *Daytime:* AMP-affiliated driving schools; private, semi-private and team track rentals (9:00am – 2:00pm)
- *Evening:* Competition vehicle testing. (2:00pm – 9:00pm).

**Friday:**

- AMP-affiliated driving schools; private, semi-private and team track rentals; competition vehicle testing and/or Competition Motorsports Events and/or Non-Competition Events. (9:00am – 11:00pm).

**Saturday, Sunday & National Holidays:**

- AMP-affiliated driving schools; private, semi-private and team track rentals; competition vehicle testing and/or Competition Motorsports Events and/or Non-Competition Events. (9:00am – 11:00pm).

**8. Capacity Expectations**

Based on the past conditions of approval for the facility, the maximum allowable occupancy was established at 8,000 persons. This number includes all spectators (guests), competitors, teams and crew members, employees, service providers and vendors. AMP plans to continue operations utilizing this 20-year old established maximum. Table A-1 provides the anticipated attendance based on the various types of events.

**Table A-1: Estimated Persons Per Event Type (Including Employees)**

<b>Events</b>	<b>Persons</b>
Standard Concert Event (Weekend)	Up to 3,000
Non-Feature Racing Event (Weekend)	Up to 4,000
Feature Concert Event (Weekend)	Up to 6,500
Feature Racing Event (Weekend)	Up to 7,000

The following information is provided to give an idea of the attendance at past events at the facility. The information for the period since the present owner acquired AMP gives a more accurate measure of attendance at events.

Average 2007 attendance (to-date)  $\approx$  2,100 persons per day, per event

- 2007 high attendance = 4,000 persons; low attendance 590 persons

Average 2006 attendance  $\approx$  1,000 persons per day, per event

- 2006 high attendance = 3,800 persons; low attendance 75 persons

Average 1996-2005 attendances unknown

- 1996-2005 high attendance = 6,500 persons; low attendance unknown

AMP 2007 average employees on-site (per event) = 42

AMP 2006 average employees on-site (per event) = 68

AMP 2006 average participants (pit attendance) on-site (per event) = 318

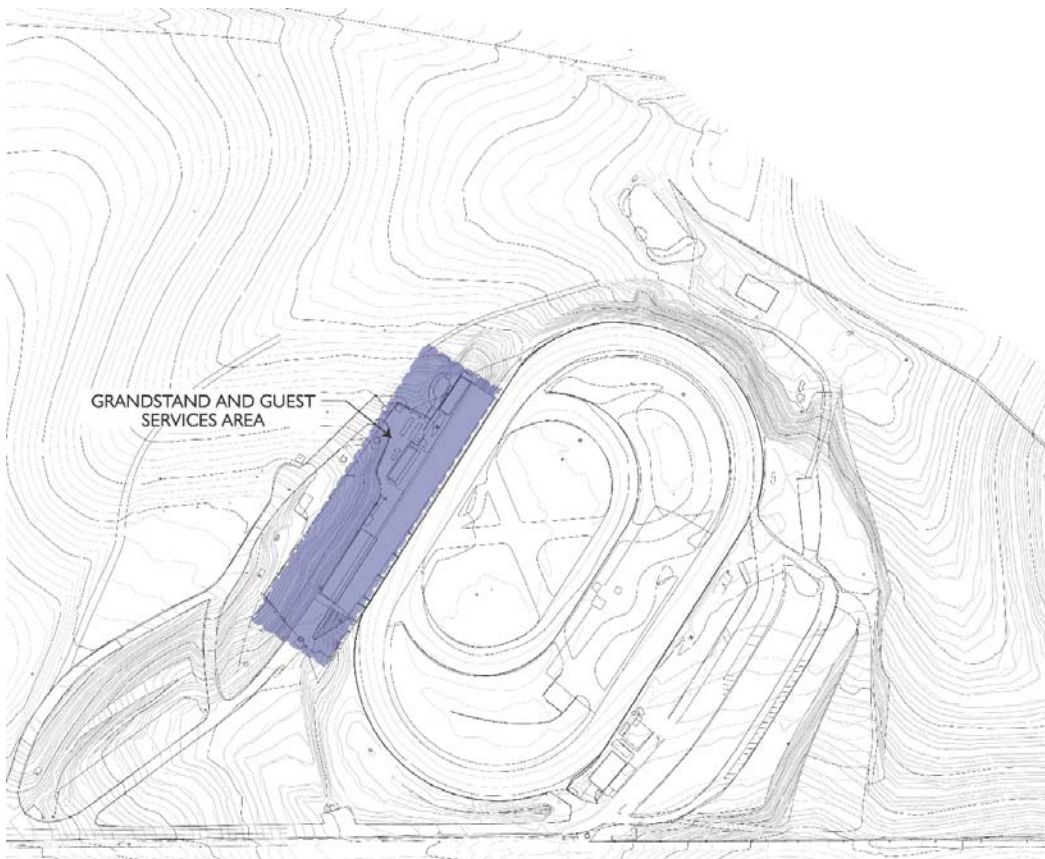
AMP 2006 average participant car counts (all events) = 46 vehicles

Types of vehicles --- Average per specific event (2006):

- o Stock cars / Modifieds – 21
- o Sprint cars / Midgets – 37
- o Trucks – 10
- o Motorcycles – 23
- o Sports Cars / Autocross / Drifting– 28
- o Mini-cars / Legends – 17

**9. Grandstand and Guest Services** – The following uses and activities are extant and will continue to be located in this area:

A) Centrally located and common to the Primary Track are the main grandstands (existing structure; see Figure 4) with a present seating capacity of 6,800 persons. Additional grandstands may be added in the future; however, the total capacity on-site will not exceed 8,000 persons, as previously allowed by the 1996 Conditional Use Permit and as currently proposed.



**Figure 4 - Grandstands and Guest Services Area**

- B) Guest services include:
- i. Central restrooms (existing);
  - ii. Food, beverage and souvenir concessions (existing);
  - iii. Ticket booths (existing);

- iv. Emergency and Security Office (existing);
- v. Guest relations booth (existing);
- vi. Race control offices (existing);
- vii. Press-TV-media facilities, broadcast and announcers booth (existing);
- viii. VIP suites located in the race control tower on the west side of the Primary Track (existing);
- ix. AMP Management Offices (existing);
- x. AMP Pit-based Track Operations Offices (existing); and,
- xi. Two stand-alone, covered pavilions to provide for shade cover (existing).

As part of the operation of AMP, the above areas will be maintained, upgraded and/or modernized, as appropriate or as required to meet State or local codes. In addition, AMP is proposing to complete an additional grandstand shade cover previously approved by the County in 1996, but never constructed. The new grandstand cover would include wind protection measures, shade and “vapor-misters” for spectator comfort in the grandstands. Construction plans will be submitted to the County for approval prior to erection.

**10. Race Operations and Facility Support Shop** - “Race Operations” for the Primary Track are currently located within the Control Tower building, located west of the Primary Track. This includes offices for track announcers, race control, emergency coordination, and timing and scoring. Technical inspection, race communications, and emergency services personnel currently operate from the pit area side of the Primary Track, which also features a Track Operations Office, Staff Lounge and pit-based concession stand. These uses will continue.

In addition, a variety of transient and/or fixed motorsports-related businesses and service providers are necessary to meet the needs of both AMP’s guests and motorsports participants. Depending on the service provided and target market (guest/competitor) these businesses are currently located within either the Guest Services or the Pit/Paddock areas.

The race support shops and driving/technical schools are located in the Pit/Paddock area. Race services have and will continue to include fuel sales, tire sales and service, component and parts sales, and safety equipment sales and service for competitors. To provide fuel to serve the various motorsports competitors, AMP has contracted with an outside vendor who dispenses fuel from stored and secured containers, meeting State and Federal regulations regarding safety and fire protection. Currently and as proposed for future operations, less than 500 gallons of racing fuel is centrally stored on-site at any time.

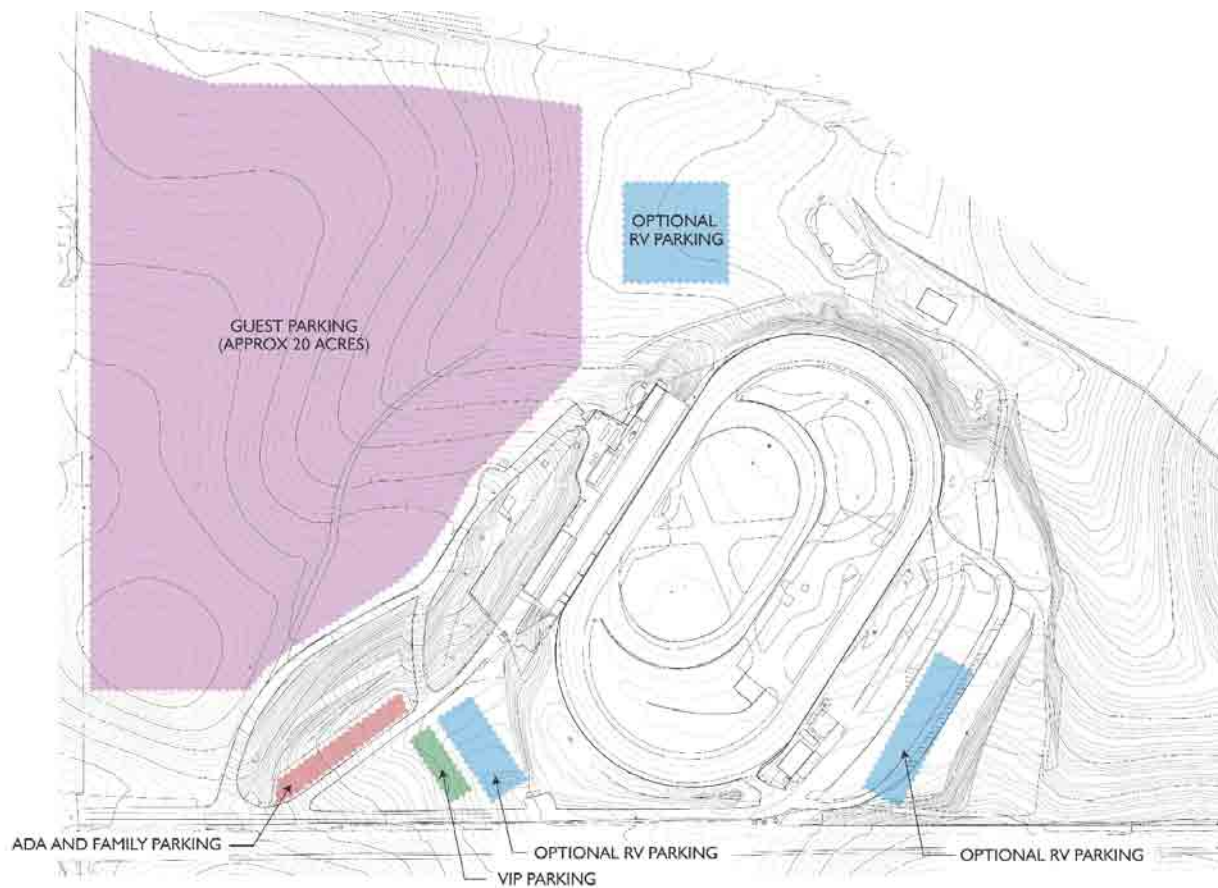
AMP’s Facilities & Maintenance Operations are also located within the Pit/Paddock Area, housed within an existing 2,000 square foot Quonset hut. The building provides warehousing of materials and equipment, and provides maintenance support for AMP’s operations. These uses will continue.

**11. Guest Parking & Buffer Area** - West of the Primary Track, Grandstand and Guest Services areas and extending to the western property boundary and located within the rolling topography of the site, is the primary “Guest Parking Area,” encompassing an area of approximately 40 acres.

Based on the attendance at most events, guest parking can be adequately handled in an area of approximately 15-20 acres; however, given larger events, and additional 10-15 acres

of the open grass area may be required for additional parking. The area most commonly used for parking (est. 15 acres) is regularly mowed to prevent grass fires from hot exhaust systems. The remaining 25 acres is mowed or attended to as required for the larger events, remaining primarily as an unimproved open-space buffer zone.

Based on an industry average 350 s.f. per parking space (including accessway), and utilizing a proven 3.2 persons per vehicle (Average Vehicle Occupancy), 20-25 acres is more than sufficient to hold an estimated 2,500 passenger vehicles that can accommodate approximately 8,000 persons (see Figure 5).



**Figure 5 – Guest Parking Area**

In addition to the primary Guest Parking area, AMP features a separate 80-car ADA parking lot, a 150-car family-services parking lot (stroller accessible), three (3) optional RV parking areas, 150-car employee parking area, and 75-car VIP Guest parking area. This area is located at the southern end of the Primary Track (see Figure 5). On occasion, motorhomes and recreational vehicles are parked for overnight stays at the northeastern end of the guest parking lot, providing superior noise, line of sight and lighting buffer from any surrounding vantage point. Competitors who utilize RV's for towing race car trailers typically park their motorhome adjacent to their race trailer within the pit area. As a final option, some RV's prefer parking trackside at Turn 1 of the ½-mile paved oval. In all cases, RV's and

motorhomes are required to be 100% self-contained. No RV services are provided on-site. Additional information on RV's and motorhomes is contained in Section 13.

Along the western boundary of the parking area, the western-most extension of the site, a 50 foot strip is designated as a buffer area to restrict guest parking in proximity to the residences located beyond the crest of the western hillside.

**12. Additional Operational Parameters and Restrictions** - The following operation parameters shall be followed, which conform to the conditions as specified in the 1996 conditional use permit:

- a. A Competition Motorsports Event may only occur on Friday, Saturday or Sunday or on any national Holiday of a given week within the specified operating hours.
- b. Non-Competition, Non-Spectator and Private Use Events may occur on any day within the approved operating hours of the facility, provided that no more than 3 Non-Competition, Non-Spectator or Private Use Events may occur during any given week within the specified operating hours.
- c. A week is defined as 12:01am Monday through 12:00pm Sunday.
- d. Notification will be provided to the Planning Department and Sheriff's Department a minimum of two weeks in advance of all events at the facility, describing event type, hours, and expected attendance; and,
- e. Some events may extend past 10:00pm Wednesday through Sunday, as a result of required operations, emergencies or unplanned operational delays; however, an 11:00pm curfew shall be maintained whereby all on-track racing activities shall cease.
- f. No race car or competition vehicle may start its engine after 11:00pm or before 8:00am. No vehicles shall be allowed on any race track prior to 9:00am.
- g. Under no circumstance shall noise levels exceed 65 dBA at any property line after 12:00pm or before 7:00am.
- h. All Primary Track lights shall be turned off within 30 minutes of the termination of the final scheduled event or activity. Grandstand lights shall be turned off within 60 minutes of the termination of the final scheduled event or activity, provided that all guests have left the primary grandstand area and that it is safe to do so. Pit and paddock area, pedestrian walkway and/or parking lot lights shall be turned off within 2 hours of the termination of the final scheduled event or activity, provided that all competitors and/or guests have left these area(s) and that it is safe to do so. All other lighting that is not required for the safe egress of spectators or staff, or the safe shut down of on-going operations shall be turned off within two hours of the termination of the final scheduled event or activity.
- i. All concert music and/or recorded music and/or the use of the public address system shall cease within 30 minutes of the termination of the final scheduled motorsports event unless a concert or follow-on event was specifically scheduled to follow the motorsports event or other activity; however, under no circumstances shall live or pre-recorded music extend later than 11:00pm.
- j. There shall be no motorcycle or other vehicle riding outside of the Primary Track, parking, and/or pit areas except for facility maintenance vehicles and equipment. Maintenance vehicles, in the performance of their normal maintenance duties on-site, are exempted from this prohibition.

**13. Recreational Vehicles** - The overnight parking of RVs and motorhomes will be accommodated based on:

- a. A maximum of 100 RVs and/or motorhomes will be allowed to park on-site for overnight stays between 9:00am Friday and 10:00am Monday, with the exception that if Monday is a National Holiday, then 10:00am Tuesday;
- b. AMP will provide a written request and application for an annual use and sanitation plan for review and approval by the Alameda County Planning Department and Alameda County Health Department;
- c. RV and/or motorhome parking will only be allowed in conjunction with a scheduled event or activity;
- d. RV generator use is allowed provided that no generator noise may exceed 65 dB when measured from a distance of 100' in any direction of the generator;
- e. All such vehicles must be 100% self-contained for water, wastewater, electrical service and/or propane; and,
- f. No dumping of wastewater shall be allowed.
- g. All exterior lights must be turned off and all exterior music must cease as of 11:00pm.
- h. Small, personal BBQ's are allowed for use only by motorhomes and/or RVs parked within the Pit and Paddock Area. No wood burning BBQs are allowed. Charcoal is allowed; propane use is highly advised. All BBQ's must have a cover to prevent flying embers. No fire rings or open flames are allowed. BBQ use in the Pit and Paddock Area may be restricted by AMP or Alameda County due to high-winds at any time.

**14. Residential Uses**

Residential uses for security and/or caretaker staff shall be allowed and will be subject to the following:

- a. A maximum of two (2) mobile and/or modular caretaker's homes, consistent with the provisions of the 1996 Conditional Use Permit;
- b. The location of the two mobile and/or modular homes shall generally be as shown (see Figures 6) with the maximum size of each home being 2,800 square feet; and,
- c. No residential uses are proposed as part of this application. The locations shown on Exhibit A are the general locations considered appropriate for future installation of the caretaker residences. A separate application may be submitted for the two mobile and/or modular caretaker homes at a future date. The present application is seeking conceptual approval of two future caretaker residences.



**Figure 6 – Caretaker Modular/Mobile Home Sites**

**15. Acoustical Standards** - The following acoustical (noise) standards have been and will continue to be enforced at AMP:

- a. During approved operating hours, a maximum of 95 dBA shall be maintained when measured at a 100-foot radius from any point of the perimeter of the defined “Primary Track” (this standard currently exists).
- b. Under no circumstance shall noise levels exceed 65 dBA at the property line after 12:00am or before 7:00am. No race car or competition vehicle that is not street legal may start its engine after 11:00pm or before 8:00am. No vehicles shall be allowed on any race track prior to 9:00am on any day (this standard currently exists).
- c. Acceptable (calibrated) noise monitoring equipment shall be maintained on-site and utilized per event to monitor and maintain applicable noise standards (this standard currently exists).
- d. Noise monitoring equipment shall be operated by an AMP staff member or hired consultant that is familiar with the operation and calibration techniques of the specific equipment (this standard currently exists).
- e. Noise logs shall be maintained and kept per event for a period of 18 months from any event; such logs shall designate the date and time of the event and all readings taken during that event, including location, type of vehicle, description of vehicle, etc.

**16. Signage** – Signage for AMP shall consist of the following:

- a. **Directional Signs** - Four (4) off-site local directional signs indicating the proper accessway to the facility. (These sign currently exist.) Specifications include:

- i. Signs will be 4' x 8' in size
  - ii. The base of the signs shall be no more than six (6) feet from ground level.
  - iii. Signs shall not be illuminated.
- iv. The location for each of the four signs include:
- Grant Line Road off-ramp / I-580 Westbound at Grant Line Road
  - Grant Line Road off-ramp / I-580 Eastbound at Grant Line Road
  - Grant Line Road at Midway Road
  - Mountain House Parkway at Midway Road.
- b. **Primary Electronic LED Sign** - One double-faced or two single-faced programmable identification and electronic reader board signs and/or other illuminated sign used to:
- o Display the identify of the facility
  - o Provide promotional messages regarding:
    - 1. The facility
    - 2. Current or upcoming events
    - 3. Event sponsors
  - o Provide commuter road and traffic information; Amber Alert status; other Public Service Announcements to east- and west-bound I-580/I-205 commuter traffic.
- i. Includes one each non-illuminated sign located directly below the illuminated sign with dimensions not to exceed that of the illuminated sign.
  - ii. Subject to final design approval by the Planning Director prior to the issuance of a building permit, further consistent with the 1996 Conditional Use Permit conditions of approval, AMP is authorized to construct and maintain one double-face sign display or two single-face sign displays. A permit for the sign shall be obtained from Cal-Trans.
  - iii. Sign cabinet dimensions shall be an industry standard of 14' tall x 48' wide.
  - iv. The height of the top of the sign cabinet shall be no more than 75 feet above the ground level at placement.
  - v. Based on a 20mm pixel placement, the digital sign is specified by the manufacturer to have an NIT rating (color corrected to 6500K white) of 9,768. (An NIT rating is a measurement of direct light; whereas a lumen rating is a measurement of reflected light.) AMP's sign shall be dimmable to 1% of full brightness for nighttime operations.
  - vi. Sign displays and LED technology shall be designed such that lighting intensity shall be automatically diminished by a minimum 50% after 10:00pm.
  - vii. Sign location shall be situated within 200' of the north-western corner of the property along the northern property boundary (see Figure 7 below – neither sign nor elevation is to scale – for location reference only) and within 50' of the northern property boundary (closest to the freeway).



**Figure 7 – Primary Electronic LED Sign Location**

- c. **Secondary Electronic LED Sign** - One single-faced programmable identification and electronic reader board signs and/or other illuminated sign used to:
  - o Display the identify of the facility
  - o Provide promotional messages regarding:
    1. The facility
    2. Current or upcoming events
    3. Event sponsors
  - o Provide commuter road and traffic information; Amber Alert status; other Public Service Announcements
- viii. Includes one each non-illuminated sign located directly below the illuminated sign with dimensions not to exceed that of the illuminated sign.
- ix. Subject to final design approval by the Planning Director prior to the issuance of a building permit and a permit for the sign shall be obtained from Cal-Trans.
- x. Sign cabinet dimensions shall be an industry standard of 14' tall x 48' wide.
- xi. The height of the top of the sign cabinet shall be no more than 50 feet above the ground level at placement.
- xii. Based on a 20mm pixel placement, the digital sign is specified by the manufacturer to have an NIT rating (color corrected to 6500K white) of 9,768. (An NIT rating is a measurement of direct light; whereas a lumen rating is a measurement of reflected light.) AMP's sign shall be dimmable to 1% of full brightness for nighttime operations.
- xiii. Sign displays and LED technology shall be designed such that lighting intensity shall be automatically diminished by a minimum 50% after 10:00pm.
- xiv. Sign location shall be situated within 1,500' of the north-western corner of the property and the Primary Electronic LED Sign, and within 50' of the northern property boundary. This sign is required to service the newly routed I-580 by-pass, currently under construction.



- d. **Timing & Scoring Sign** - One electronic, semi-programmable timing, lap count and scoreboard (this sign currently exists).
  - i. Sign features a blank “billboard” backing that is not illuminated (extant)
  - ii. Sign is approximately 20 feet x 30 feet
  - ii. Sign is located at the north end of Primary Track and may be relocated to the east side of the Primary Track, opposite grandstand area
  - v. Sign lighting is coordinated such that all lights are turned off when the event track lighting is turned off.
  
- e. **Trackside Promotion Billboards** - Direct illuminated billboard promotional signs (these signs currently exist).
  - i. Twenty-four (24) direct illuminated billboard promotional signs
  - ii. Signs are 8 feet by 20 feet
  - iii. Signs are not be visible from off-site, with the exception of one residence located to the south of AMP (due to the elevation and situation of the residence)
  - iv. Lighting is designed and sited to prohibit direct orientation outside of the Primary Track area.
  - v. Lighting is coordinated such that all lights are turned off when the event track lighting is turned off.

- e. **On-site Banners and Vendor Signs** - Signs and advertising (lot) visible only to on-site spectators not to be regulated, consistent with the 1996 Conditional Use Permit conditions of approval (these signs currently exist).

**17. Lighting Plans** – The design concept for lighting is presented in two distinct components; *Site Lighting* and *Track Lighting*.

- a. Site Lighting addresses those non-Primary Track portions of the site, such as the Guest Services and Grandstand Area, pedestrian walkways, buildings, parking areas and internal roads.
- b. Track Lighting covers the Primary Track Area and related facilities (Pit/Paddock Area); areas that must be lit for competitor and staff safety.

AMP's lighting program has been designed and installed to minimize the glare and spillage created on the project site, as prescribed in the Alameda County Zoning Code requirements. Safety and security are foremost concerns. As part of the lighting program the following guidelines have been included:

**Site Lighting** – Site lighting is to be contained within the immediate area where it is utilized. Light that can escape into the night sky will be kept to a minimum by using engineered and proven fixture sources with environmental spill and glare control. While direct lighting or light sources will not be seen from the adjacent area, an indirect glow is anticipated from light reflected from moisture in the air.

**Parking** – Parking area lighting will be consistent with County codes for parking lot areas (applicable to rural areas) and conventional security standards in order to create a situation that is safe for all people, including women and young families.

**Guest Services Area** – Localized path lighting has been installed under the Main Pavilion and on all main pathways to illuminate the main walkway. (All lighting currently exists.)

**Pit and Paddock Area** – Mixed 400- and/or 1000-watt metal halide fixtures mounted on 30-foot wooden poles is currently in place. (All lighting currently exists.)

**Track Lighting** – The lighting system within the Primary Competition Area is designed to meet the needs of the track's most common racing types, as well as being television broadcast quality. To meet the varying needs of AMP's various racing events, lighting design is largely based on four criteria: (1) sanctioning body standards for participant safety; (2) spectator safety and event visibility; (3) television quality; and (4) reduction of off-site spill and glare. To reduce spill and glare, the pole heights vary with the setback to maintain 20 degrees or more aiming angles. If the number of poles is reduced, typically the height of the poles is increased. Conversely, if the height of the poles were reduced, the number of poles would need to be increased. All track related lighting currently utilizes 1500-watt halide fixtures with Level 8 glare control visors.

System design was coordinated by Musco Sports Lighting Company. A Musco "California Professional Engineer" has certified all pole and foundation installations. Rex Moore Electrical Contractors certified all wiring and electrical terminations.

AMP's track lighting has been designed to meet all current media coverage standards. If additional "portable" lighting is required for a particular media event, it would be localized in nature and, as such, would not exceed any level of significant impact (glare, spillage, or intensity) beyond the ambient lighting conditions allowed for the facility as a whole.

Outdoor lighting exists in the locations as shown on Lighting Plans. Exhibit E, Sheets 1-5 provides a Lighting Diagrams of the existing illumination levels for the Primary Track Area.

Table A-2 describes the lighting fixtures for the various parts of the AMP. A general discussion of these areas is also provided below.

**Table A-2. Lighting Characteristics and Standards for the AMP Project**

Location	Lighting Fixture (number of fixtures)	Max. # of Poles	Max. Pole Height	Light Intensity (foot candle)	
				Minimum	Maximum
<b>Site Lighting</b>					
ADA + VIP + Turn 1 RV + General Parking Areas	Low mast lighting using wooden poles and extant 1000-watt fixtures	12	30 feet	1	10
Guest Services Area	Fixed Hubbell SLS Sportsliter 1500-watt halide fixtures with Level 8 glare control visors	0	Fixed Lights	3	20

<b>Track Lighting</b>					
Primary Competition Area	1500-watt halide fixtures with Level 8 glare control visors	12	90 feet	17	35
Pit & Paddock Area	1000-watt halide fixtures	6	30 feet	3	24

**18. Utilities** – All utilities at the site currently exist.

**A) Potable Water** –

AMP has maintained the facilities to provide potable water in sufficient quantity to furnish water for the maximum 8,000-person capacity from an existing on-site well, subject to meeting the performance standards of the Alameda Department of Health Services. If, at any time, the on-site well does not meet the required performance

standards, then potable water will be imported to an on-site storage tank that meets the storage capacity requirements of the County.

**B) Emergency water supply –**

Emergency water will be solely from an existing on-site water well and storage tank(s). Storage capacity is sufficient to meet Alameda County Fire Department requirements.

**C) Non-Potable Water –**

Non-potable water used for minimal irrigation, dust control and fire suppression. It is currently and will continue to be supplied by one of the existing wells on the site.

**D) Wastewater –**

The existing on-site septic system and leach field will remain. The site also includes self-contained restroom trailers and port-o-lets as needed to meet capacity needs of the site. Services are performed by pump and tank trucks; mid-week between 9:00am - 5:00pm.

**E) Stormwater Drainage –**

The project site has been graded to provide positive flow paths in order to efficiently collect and convey stormwater runoff to a general detention pond for percolation and/or conveyance by Caltrans drainage systems adjacent to and servicing I-580. This pond has been tested and noted in various Alameda County permits. Given that the facility's parking lot and all internal areas of the various racetracks consist of open-ground, no stormwater collection is required.

**F) Electricity –**

PG&E has and will continue to serve the electrical service for the project site.

**G) Propane –**

The project has and continues to use on-site propane tanks for concession stand use and gas supply. There will be no natural gas service to the site. Mid-week propane service between 9:00am - 5:00pm.

**H) Solid Waste –**

Tracy Disposal Service supplies refuse and solid waste removal service to the site. Mid-week refuse removal between 9:00am - 5:00pm.

**I) Telecommunications –**

Telecommunication services at the site will continue to be supplied by AT&T.

Table A-3 provides a breakdown of anticipated Average Daily Demand and Yearly Demands for water, wastewater and electricity.

**Table A-3: Utility Build-out Thresholds**

<b>Utility</b>	<b>Average Daily Demand<sup>1</sup></b>	<b>Yearly Demand<sup>1</sup></b>
<b>Water</b>		
Potable	.0006 mgd <sup>2</sup>	.25 mgy <sup>2</sup>
Non-Potable	.002 mgd <sup>2/3</sup>	0.73 mgy <sup>2/3</sup>
<b>Wastewater</b>	0.0005 mgd <sup>2</sup>	0.183 mgy <sup>2</sup>
<b>Electricity</b>	50 KWh/d <sup>4</sup>	18,375 KWh/y <sup>4</sup>

<sup>1</sup> Demand based on a season of 40 weeks; 30 weekends of non-feature events and 10 weekends of Feature Events. Total demand and generation includes identified events plus all other uses/functions occurring on-site.

<sup>2</sup> mgd = million gallons per day; mgy = million gallons per year

<sup>3</sup> Includes site irrigation and dust control

<sup>4</sup> KWh/d = kilowatt hours per day; KWh/y = kilowatt hours per year

**19. Landscaping** – AMP’s landscaping has been focused on aesthetic shrub and tree planting at the main entry driveway and the two main entry walkways. All landscaping exists. No other extensive landscaping is planned for the facility; however, the facility may benefit from the planting of additional trees and shrubs in the future. No additional irrigation is planned for the facility.

**20. Fencing** – Various forms of fencing are utilized within the AMP project for security, designated access areas, public safety, motorsports safety, crowd control and to permit the safe, restricted migration and travel of wildlife through the site. All fencing currently exists.

Safety and security are critically important. Security Fencing protects the perimeter of all core motorsports venues within the site. This consists of between 6 and 12 foot tall cyclone fencing.

Standard cyclone fencing has been used externally and internally throughout the facility where permanent fencing is required for security, safety and crowd control. Inside the facility, cyclone fencing is used to separate pedestrian walkways and other spectator areas from race venues and their associated facilities, maintenance and construction areas, security areas, and service roads. These internal fences vary between a minimum of 4 feet and a maximum of 12 feet in height. Exterior and interior cyclone fencing traps blowing refuse and errant trash, thereby, limiting the amount of waste traveling off-site.

White split-rail or agrarian style, ornamental fencing has been utilized along the parking lot (including RV and ADA areas) to demarcate the parking facility.

In some cases, venue access and security fencing will need to be flexible to allow for multiple configurations. Temporary fencing and barricades may be utilized for crowd management as needed.

Safety Catch Fencing has also been provided around the Primary Track, between the racing surface and the general spectator area. The precise design of this fencing is mandated by the current standards of the sanctioning bodies and insurance providers. All fencing currently exists.

**21. Paving and Impervious Surfaces** – Given that AMP’s core business requires asphalt, concrete and/or other paved surfaces for competition, pit area use, ancillary and incidental events, parking, and public walkways, and that the maintenance of these paved or impervious surfaces are required for both competitor and public safety, as well as to maintain proper operating conditions, AMP has and will continue to pave and/or re-pave certain areas of the Primary Track, Pit and Paddock Area, Grandstands and Guest Services Area, and any other area which is currently paved or concreted, or has been paved or concreted in the past. This includes any area which may require the repair, replacement, resealing or application of new pavement to maintain safe or proper operations, or to meet those conditions required by Federal or State law, or meet the terms of AMP’s operating agreement with Alameda County.

**22. Public Services** - AMP has and will continue to provide adequate public services to ensure the security and safety of spectators, employees, event participants and the immediate surrounding community.

**A) Law Enforcement / Emergency Services** – The Alameda County Sheriff’s Department (ACSD) has and will continue to provide Law enforcement services. The nearest Sheriff’s station to the site is located in Livermore. It is anticipated that no additional law enforcement personnel will be required to maintain proper levels of service for AMP events; however, for particularly large, feature events additional law enforcement services may be requested and contracted. To establish an appropriate level of on-site security needed during AMP events, AMP may contract with ACSD or provide private security from a trained, licensed and bonded company to supplement the services provided by the ACSD.

AMP has prepared a comprehensive Emergency Response Plan (ERP) for approval by the Alameda County Sheriff’s Office, Alameda County Office of Emergency Services, Alameda County Fire Department, Alameda County Department of Public Health, California Highway Patrol, and Caltrans. The ERP addresses potential safety hazards associated with the project and identifies public safety hazards that can be reduced or eliminated through specific protocols. The ERP also provides an overview of general procedures required to protect people and property during an emergency or disaster situation. The intent is to establish a clear understanding of responsibilities for first responders, sheriff and police, local fire departments, emergency medical service agencies, and track management staff during an emergency situation.

The ERP identifies and assigns personnel to various emergency tasks and responsibilities, thus creating a “Site Emergency Team.” The ERP describes the emergency management procedures for both on-track and off-track incidents ranging from minor, single-vehicle motorsports accidents to complex emergencies requiring a variety of outside resources. Included are designated assignments for on-site personnel, details of each position’s responsibilities, procedures for coordination with outside resources, and establishment of a chain-of-command to take precedence in emergencies.

The Emergency Response Plan uses the Incident Management System (IMS) to provide guidelines for all magnitudes of emergency situations, incorporating appropriate levels of management and resources for various incidents. Response levels are defined by the specific actions required.

Separate from, but related to the ERP, AMP will prepare an Incident Action Plan (IAP) that details action items and reporting related to any incident. More detailed than an ERP, those situations more common to a motorsports facility are identified within the IAP. AMP will prepare a comprehensive Incident Action Plan (IAP) in coordination with the Alameda County Office of Emergency Services for each event with an anticipated attendance of more than 6,000 guests (Major Feature Event) as a supplement to the ERP.

- B) Fire Protection and Emergency Medical Services** – The Alameda County Fire Department (ACFD) has and will continue to provide fire service. The closest fire station within the Alameda County Fire District is located within the City of Livermore, approximately eight miles west of the AMP project site.

Whereas services provided by Alameda County are general in nature, AMP will maintain a full fire suppression team and the necessary equipment to effectively respond to motorsports events. AMP-provided fire prevention resources and fire suppression equipment will be located on the site and will be controlled by AMP's staff during racing events. This will provide immediate fire prevention and emergency response resources within the facility. These resources could also be utilized for specific emergencies in the surrounding community, if needed. In addition, existing mutual aid agreements will support the facility.

The Alameda County Fire Department will provide first responder emergency medical service within the project area. Emergency ambulance/EMT service (contracted by AMP) and helipad access for emergency air-transport are available for all events

**23. Helicopters** – AMP will not contract helicopters in conjunction with any event at the facility. Helicopter access to the site will be restricted to those instances where emergency response agencies require the use of an existing area on-site, demarcated as a landing pad. AMP has agreements with these local emergency agencies that will allow them the use the site, as discussed above in Section 22.

#### **24. Safety and Fire Suppression**

##### **Fire –**

A fire suppression water supply, fire hydrant, supply lines, and other equipment has been and will continue to be provided by the Alameda County Fire Department.

##### **Spectator protection –**

Spectators will continue to be given all reasonable protection from racing vehicles, with AMP maintaining a liability policy in the minimum amount of \$2,000,000, with the County of Alameda named as additional insured.

##### **Security Plan –**

AMP will submit annually a Racetrack Security Plan for the review and approval of the County prior to every annual motorsports season.

#### **25. Traffic and Circulation**

A Traffic Management Plan has and will continue to be submitted to the Alameda County Planning Department and Alameda County Sheriff's Department prior to each motorsports season; the plan will include AMP's influential means to direct a minimum of 60 percent of event traffic to access the site and/or depart the site to the South, by use of Patterson Pass Road.

Spectator parking will be limited to those areas identified in Figure 5 as Guest Parking, VIP, RV and ADA Parking areas. Competitor, participant and staff parking will be limited to either the Guest Parking area or the Pit/Paddock area.

No event parking will be allowed to occur along Midway Road or outside the designated on-site parking areas. AMP will support any effort by Alameda County to change the designation of Midway Road as a permanent “No Parking” zone from Grant Line Road to Patterson Pass Road.

The access driveway from Midway Road will be maintained and kept free of potholes. The access road leading to Guest Parking Area shall be a minimum width of 20 feet. To control traffic leaving AMP, a stop sign will be maintained at the driveway entrance at Midway Road and a clear line-of-sight will be maintained from the southwest corner of property to Midway Road

## **26. Storage**

AMP utilizes portable storage (shipping) containers on-site for the storage and/or warehousing of various dry goods, equipment, machinery, maintenance and repair items, and spare parts. Two such containers are located behind the main office building. Six additional containers are located in the pit/paddock area (two serve double-duty as a team spotter station).

AMP also utilizes the downward slope of the northeast corner of the pit/paddock area, towards the property boundary, as an open storage yard for excess emergency lighting systems, excess emergency vehicles storage, large facility maintenance and parts storage (cyclone fencing, fence poles, etc.). This area encompasses no more than 3 acres in total area.

## **27. Debris and Trash Clean Up**

### **On-Site –**

All trash, car parts, paper, boards, broken glass, and other debris will be collected within 24 hours of the last race of the weekend or other event and will be disposed of properly.

### **Midway Road right-of-way –**

Within 24 hours after any event, for a distance of 1,000 yards in either direction of the property line, litter generated by users of the facility will be collected and disposed of properly.

## **List of Exhibits**

Exhibit A	Current Aerial Site Photographs
Exhibit B	1996 CUP Site Plan
Exhibit C	Existing Grading
Exhibit D	Existing Drainage
Exhibit E	Existing Lighting Plan, Sheets 1-6
Exhibit F	Existing Pit/Paddock Lighting Plan
Exhibit G	Regional Fault Map

- Exhibit A: Current Aerial Site Photographs**
- Exhibit B: 1996 CUP Site Plan**
- Exhibit C: Existing Grading**
- Exhibit D: Existing Drainage**
- Exhibit E: Existing Lighting Plan, Sheets 1-6**
- Exhibit F: Existing Pit/Paddock Lighting Plan**
- Exhibit G: Regional Fault Map**

---

**EXHIBIT A – PROJECT PHOTOGRAPHS**

---

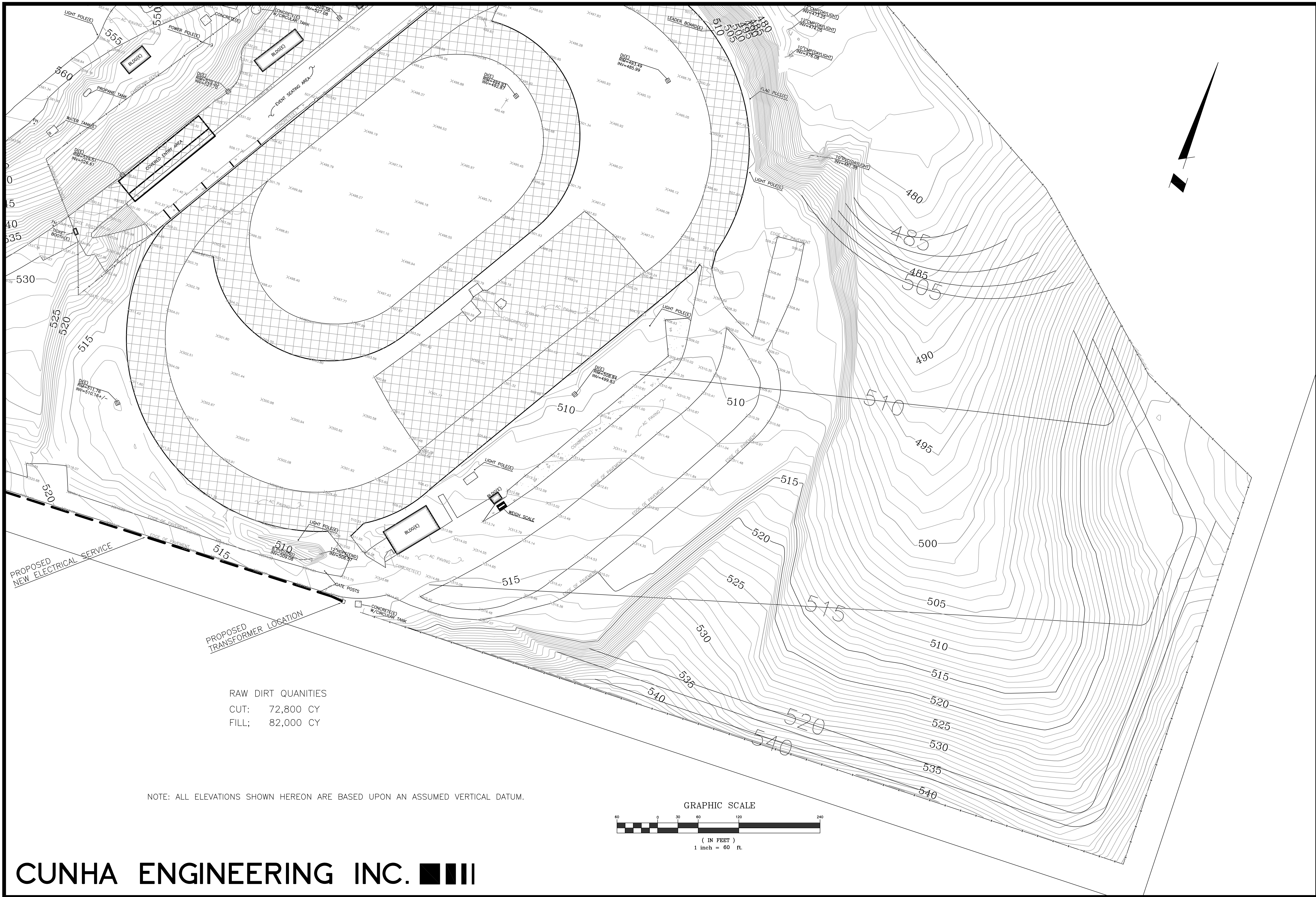




- Exhibit A: Current Aerial Site Photographs**  
**Exhibit B: 1996 CUP Site Plan**  
**Exhibit C: Existing Grading**  
**Exhibit D: Existing Drainage**  
**Exhibit E: Existing Lighting Plan, Sheets 1-6**  
**Exhibit F: Existing Pit/Paddock Lighting Plan**  
**Exhibit G: Regional Fault Map**

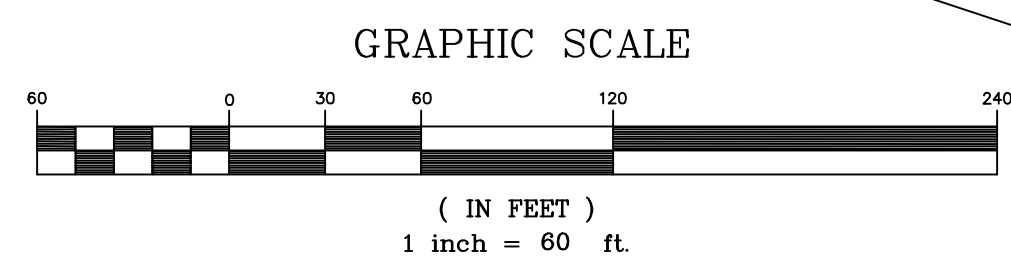


- Exhibit A: Current Aerial Site Photographs**  
**Exhibit B: 1996 CUP Site Plan**  
**Exhibit C: Existing Grading**  
**Exhibit D: Existing Drainage**  
**Exhibit E: Existing Lighting Plan, Sheets 1-6**  
**Exhibit F: Existing Pit/Paddock Lighting Plan**  
**Exhibit G: Regional Fault Map**



RAW DIRT QUANTITIES  
 CUT: 72,800 CY  
 FILL: 82,000 CY

NOTE: ALL ELEVATIONS SHOWN HEREON ARE BASED UPON AN ASSUMED VERTICAL DATUM.



**CUNHA ENGINEERING INC.** ■■■

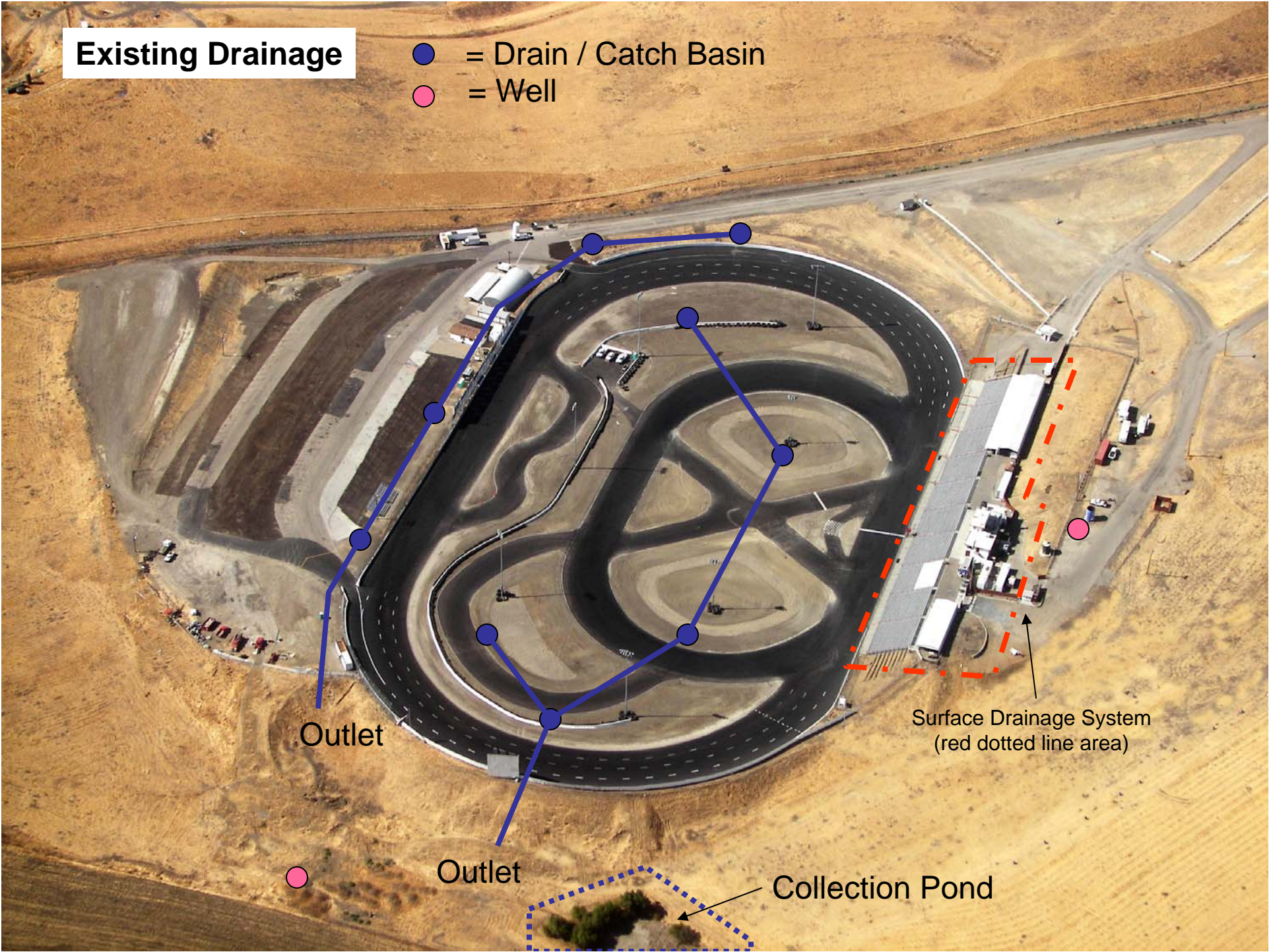
DESIGNED BY: V.H.C.	APPROVED BY: R.C.E.	DATE	DESCRIPTION
DRAWN BY: R.C.E.	CHECKED BY: V.H.C.	DATE	DESCRIPTION
CHECKED BY: V.H.C.	SCALE: 1" = 60'	DATE	DESCRIPTION
<b>CUNHA ENGINEERING INC.</b> 701 BELMONT WAY, STE. A PINOLE, CALIFORNIA 94564 (510) 741-8290		DATE	DESCRIPTION
<b>TOPOGRAPHIC SURVEY</b> <b>ALTAMONT</b> <b>MOTORSPORTS PARK</b>		CALIFORNIA	TRACY
SHEET NUMBER		3	
OF 3 SHEETS		DRAWING NO.	
206005			

<b>Exhibit A:</b>	<b>Current Aerial Site Photographs</b>
<b>Exhibit B:</b>	<b>1996 CUP Site Plan</b>
<b>Exhibit C:</b>	<b>Existing Grading</b>
<b><u>Exhibit D:</u></b>	<b><u>Existing Drainage</u></b>
<b>Exhibit E:</b>	<b>Existing Lighting Plan, Sheets 1-6</b>
<b>Exhibit F:</b>	<b>Existing Pit/Paddock Lighting Plan</b>
<b>Exhibit G</b>	<b>Regional Fault Map</b>

# Existing Drainage

● = Drain / Catch Basin

● = Well



<b>Exhibit A:</b>	<b>Current Aerial Site Photographs</b>
<b>Exhibit B:</b>	<b>1996 CUP Site Plan</b>
<b>Exhibit C:</b>	<b>Existing Grading</b>
<b>Exhibit D:</b>	<b>Existing Drainage</b>
<b>Exhibit E:</b>	<b>Existing Lighting Plan, Sheets 1-6</b>
<b>Exhibit F:</b>	<b>Existing Pit/Paddock Lighting Plan</b>
<b>Exhibit G</b>	<b>Regional Fault Map</b>

EQUIPMENT LIST FOR AREAS SHOWN											
Pole					Luminaires						
QTY	LOCATION	SIZE	GRADE ELEVATION	MOUNTING HEIGHT	LAMP TYPE	STY	THS	OTHER	STY	THS	OTHER
2	A1, A4	80'	-	80'	1500W MZ	7	7	0			
2	A2, A3	70'	28'	95'	1500W MZ	11	11	0			
2	B1, B5	90'	-	90'	1500W MZ	11	11	0			
2	B2, B4	90'	-	90'	1500W MZ	10	10	0			
1	B3	90'	-	90'	1500W MZ	8	8	0			
2	C1-C2	70'	-	70'	1500W MZ	8	8	0			
11	<b>TOTALS</b>					114	114	0			

\* This structure utilizes a back-to-back mounting configuration



GUARANTEED PERFORMANCE

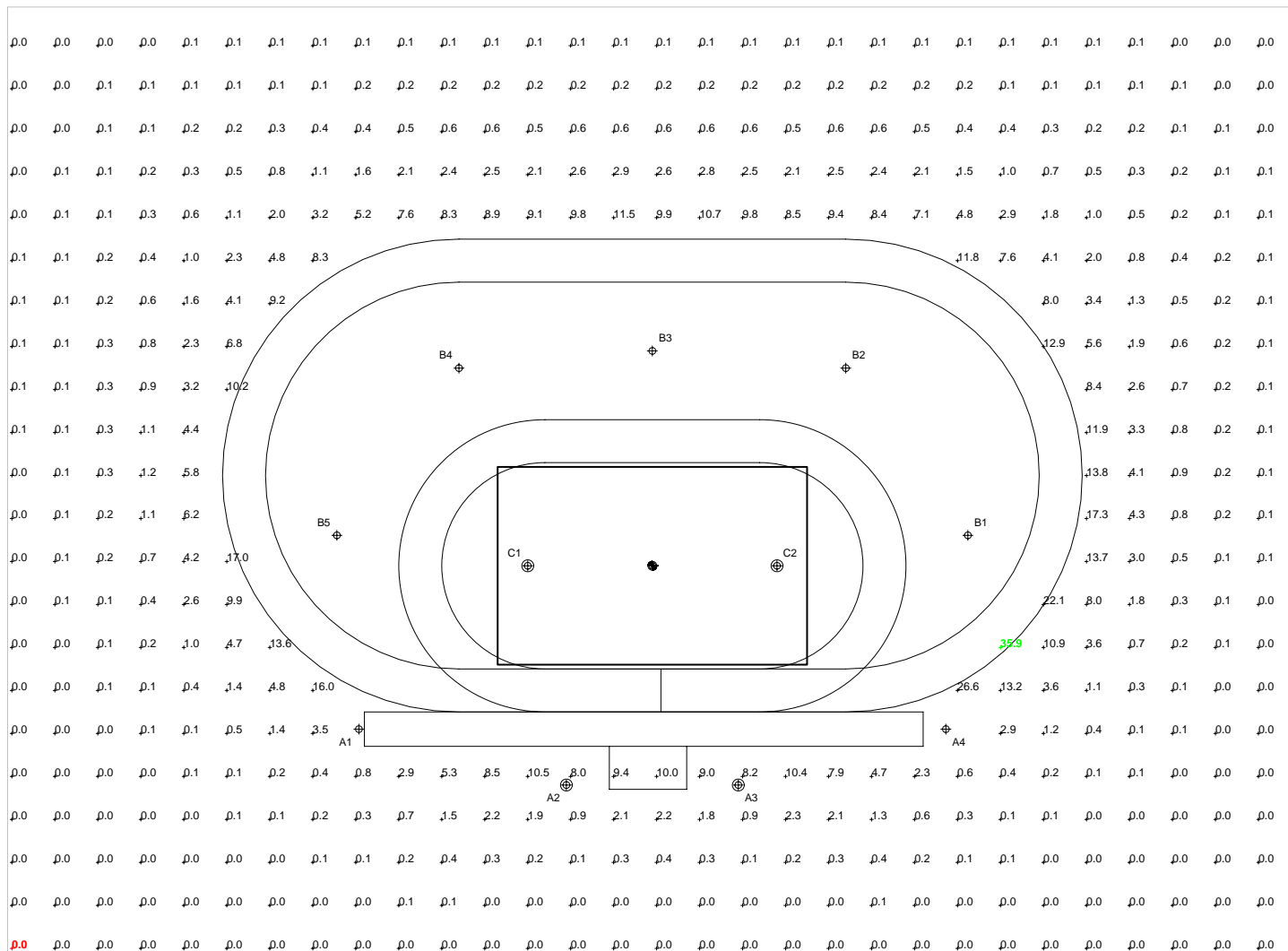
**ILLUMINATION SUMMARY**

Spill Grid  
Altamont Motorsports Park  
Tracy, CA.

Spill Grid  
- Grid Spacing = 50.0' x 50.0'  
- Values given at 3.0' above grade

- Luminaire Type: Green Generation  
- Rated Lamp Life: 5000 hours  
- Avg Lumens/Lamp: 134,000

CONSTANT ILLUMINATION HORIZONTAL FOOTCANDLES	
Entire Grid	450
No. of Target Points:	450
Average:	1.72
Maximum:	36
Minimum:	0
Avg/Min:	592.56
Max/Min:	12407.33
UG (Adjacent Pts):	12.00
UV:	2
Average Lamp Tilt Factor:	1.000
Number of Luminaires:	114
Avg KW over 5000 hours:	178.3
Max KW for Sizing Transformer:	193.8



**Guaranteed Performance:** The CONSTANT ILLUMINATION described above is guaranteed for the rated life of the lamp.

**Field Measurements:** Averages shall be +/-10% in accordance with IESNA RP-6-01 and CIBSE LG4. Individual measurements may vary from computer predictions.

**Electrical System Requirements:** Refer to Amperage Draw Chart and/or the "Musco Control System Summary" for electrical sizing.

**Installation Requirements:** Results assume +/- 3% nominal voltage at line side of the ballast and structures located within 3 feet (1m) of design locations.

SCALE IN FEET 1 : 100

EQUIPMENT LIST FOR AREAS SHOWN											
Pole					Luminaires						
QTY	LOCATION	SIZE	GRADE ELEVATION	MOUNTING HEIGHT	LAMP TYPE	STY	THS	OTHER	STY	THS	OTHER
2	A1, A4	80'	-	80'	1500W MZ	7	7	0			
2	A2, A3	70'	2#	95'	1500W MZ	11	11	0			
2	B1, B5	90'	-	90'	1500W MZ	11	11	0			
2	B2, B4	90'	-	90'	1500W MZ	10	10	0			
1	B3	90'	-	90'	1500W MZ	8	8	0			
2	C1-C2	70'	-	70'	1500W MZ	8#	14	0			
11	<b>TOTALS</b>					114	114	0			

\* This structure utilizes a back-to-back mounting configuration



GUARANTEED PERFORMANCE

**ILLUMINATION SUMMARY**

Spill Grid  
Altamont Motorsports Park  
Tracy, CA.

Spill Grid  
- Grid Spacing = 50.0' x 50.0'  
- Values given at 3.0' above grade

- Luminaire Type: Green Generation  
- Rated Lamp Life: 5000 hours  
- Avg Lumens/Lamp: 134,000

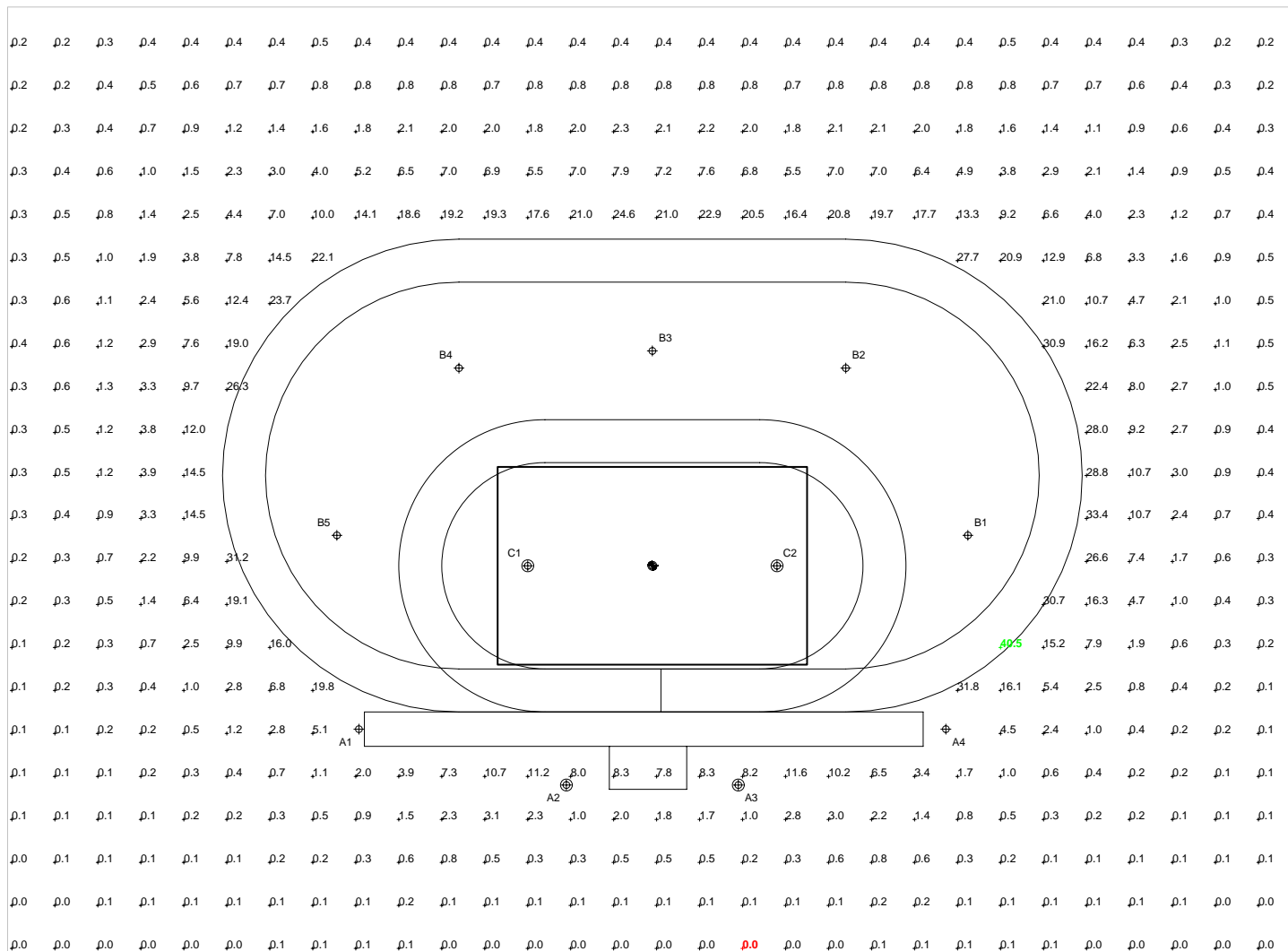
CONSTANT ILLUMINATION MAX VERTICAL FOOTCANDLES	
Entire Grid	450
No. of Target Points:	450
Average:	3.62
Maximum:	40
Minimum:	0
Avg/Min:	177.61
Max/Min:	1985.95
UG (Adjacent Pts):	8.83
CV:	2
Average Lamp Tilt Factor:	1.000
Number of Luminaires:	114
Avg KW over 5000 hours:	178.3
Max KW for Sizing Transformer:	193.8

**Guaranteed Performance:** The CONSTANT ILLUMINATION described above is guaranteed for the rated life of the lamp.

**Field Measurements:** Averages shall be +/-10% in accordance with IESNA RP-6-01 and CIBSE LG4. Individual measurements may vary from computer predictions.

**Electrical System Requirements:** Refer to Amperage Draw Chart and/or the "Musco Control System Summary" for electrical sizing.

**Installation Requirements:** Results assume +/- 3% nominal voltage at line side of the ballast and structures located within 3 feet (1m) of design locations.



EQUIPMENT LIST FOR AREAS SHOWN									
Pole					Luminaires				
QTY	LOCATION	SIZE	GRADE ELEVATION	MOUNTING HEIGHT	LAMP TYPE	STY	THS	OTHER	OTHER
2	A1, A4	80'	-	80'	1500W MZ	7	7	0	0
2	A2, A3	70'	28'	95'	1500W MZ	11	11	0	0
2	B1, B5	90'	-	90'	1500W MZ	11	11	0	0
2	B2, B4	90'	-	90'	1500W MZ	10	10	0	0
1	B3	90'	-	90'	1500W MZ	8	8	0	0
2	C1-C2	70'	-	70'	1500W MZ	8/6	14	0	0
11	TOTALS					114	114	0	0

\* This structure utilizes a back-to-back mounting configuration



GUARANTEED PERFORMANCE

**ILLUMINATION SUMMARY**

50' Spill  
Altamont Motorsports Park  
Tracy, CA.

50' Spill  
- Grid Spacing = 50.0'  
- Values given at 3.0' above grade

- Luminaire Type: Green Generation  
- Rated Lamp Life: 5000 hours  
- Avg Lumens/Lamp: 134,000

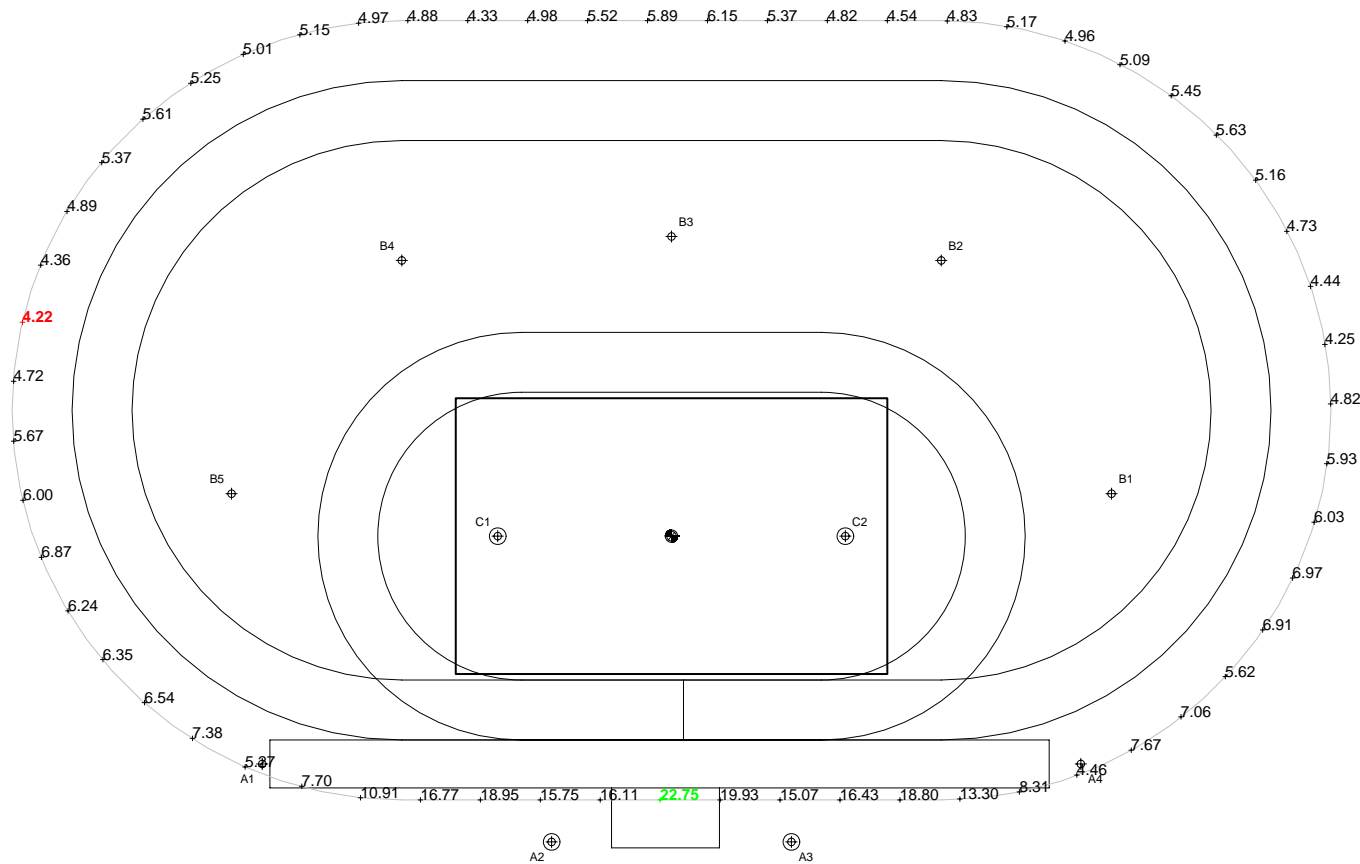
CONSTANT ILLUMINATION HORIZONTAL FOOTCANDLES	
Entire Grid	
No. of Target Points:	59
Average:	7.6665
Maximum:	22.75
Minimum:	4.22
Average Lamp Tilt Factor:	1.000
Number of Luminaires:	114
Avg KW over 5000 hours:	178.3
Max KW for Sizing Transformer:	193.8

**Guaranteed Performance:** The CONSTANT ILLUMINATION described above is guaranteed for the rated life of the lamp.

**Field Measurements:** Averages shall be +/-10% in accordance with IESNA RP-6-01 and CIBSE LG4. Individual measurements may vary from computer predictions.

**Electrical System Requirements:** Refer to Amperage Draw Chart and/or the "Musco Control System Summary" for electrical sizing.

**Installation Requirements:** Results assume +/- 3% nominal voltage at line side of the ballast and structures located within 3 feet (1m) of design locations.



Pole location(s) with dimensions are relative to 0.0 reference point(s)

By: Tim Drost Date: 29-Dec-06

File #: 25198C2  
Not to be reproduced in whole or part without the written consent of Musco Lighting. ©1981, 2006 Musco Lighting

EQUIPMENT LIST FOR AREAS SHOWN											
Pole					Luminaires						
QTY	LOCATION	SIZE	GRADE ELEVATION	MOUNTING HEIGHT	LAMP TYPE	STY	THS	OTHER	THS	OTHER	
2	A1, A4	80'	-	80'	1500W MZ	7	7	0			
2	A2, A3	70'	2#	95'	1500W MZ	11	11	0			
2	B1, B5	90'	-	90'	1500W MZ	11	11	0			
2	B2, B4	90'	-	90'	1500W MZ	10	10	0			
1	B3	90'	-	90'	1500W MZ	8	8	0			
2	C1-C2	70'	-	70'	1500W MZ	8#	14	0			
11	<b>TOTALS</b>								114	114	0

\* This structure utilizes a back-to-back mounting configuration



GUARANTEED PERFORMANCE

**ILLUMINATION SUMMARY**

50' Spill  
Altamont Motorsports Park  
Tracy, CA.

50' Spill  
- Grid Spacing = 50.0'  
- Values given at 3.0' above grade

- Luminaire Type: Green Generation  
- Rated Lamp Life: 5000 hours  
- Avg Lumens/Lamp: 134,000

**CONSTANT ILLUMINATION  
MAX VERTICAL FOOTCANDLES**

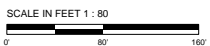
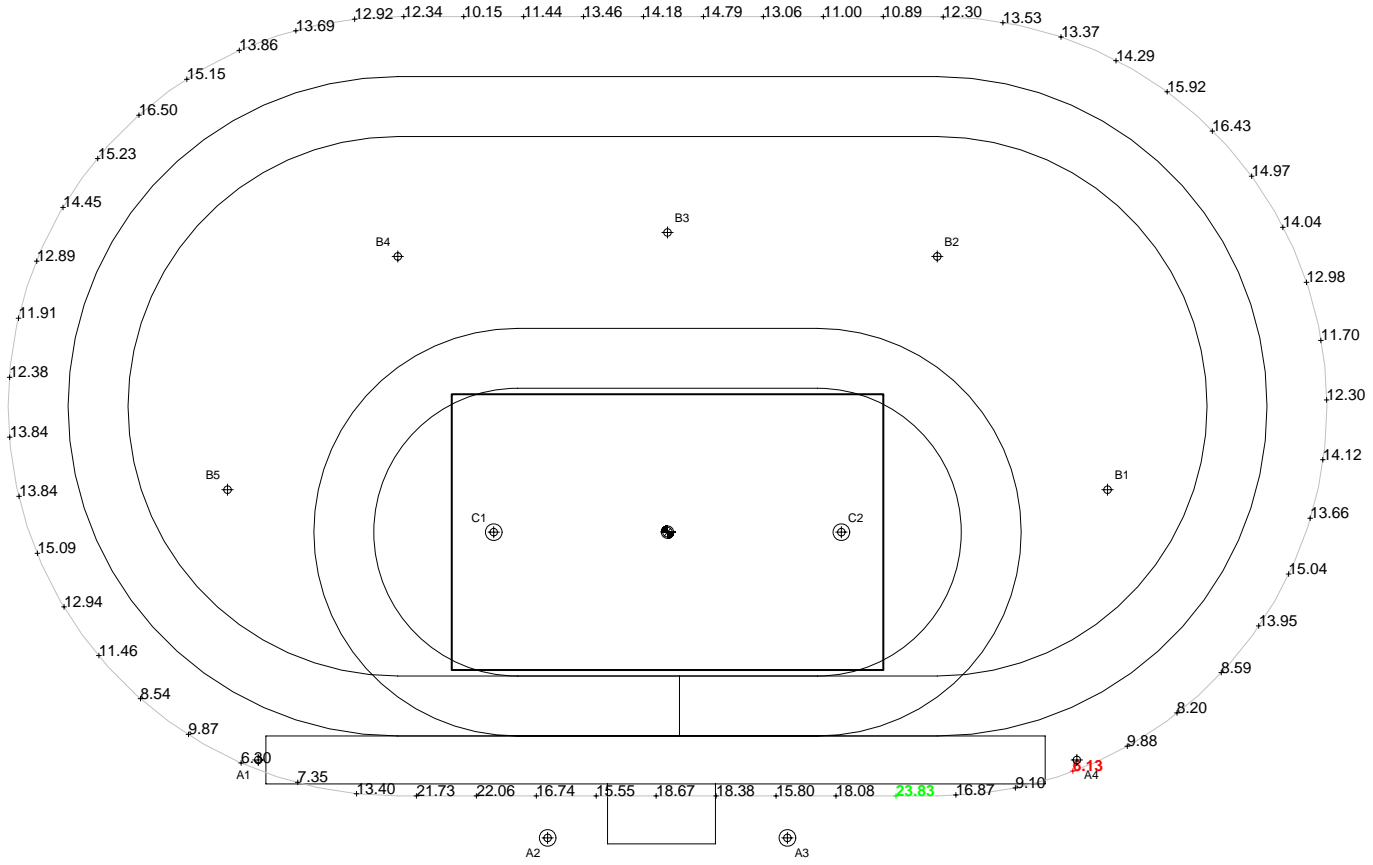
Entire Grid	
No. of Target Points:	59
Average:	13,578.3
Maximum:	23.83
Minimum:	6.13
Average Lamp Tilt Factor:	1.000
Number of Luminaires:	114
Avg KW over 5000 hours:	178.3
Max KW for Sizing Transformer:	193.8

**Guaranteed Performance:** The CONSTANT ILLUMINATION described above is guaranteed for the rated life of the lamp.

**Field Measurements:** Averages shall be +/-10% in accordance with IESNA RP-6-01 and CIBSE LG4. Individual measurements may vary from computer predictions.

**Electrical System Requirements:** Refer to Amperage Draw Chart and/or the "Musco Control System Summary" for electrical sizing.

**Installation Requirements:** Results assume +/- 3% nominal voltage at line side of the ballast and structures located within 3 feet (1m) of design locations.



Pole location(s) Ⓢ dimensions are relative to 0.0 reference point(s) Ⓢ

By: Tim Drost Date: 29-Dec-06  
File #: 25198C2

Not to be reproduced in whole or part without the written consent of Musco Lighting. ©1981, 2006 Musco Lighting

EQUIPMENT LIST FOR AREAS SHOWN											
Pole				Luminaires							
QTY	LOCATION	SIZE	GRADE ELEVATION	MOUNTING HEIGHT	LAMP TYPE	STY	THS	OTHS	OTHER	GRIDS	
2	A1, A4	80'	-	80'	1500W MZ	7	7	0			
2	A2, A3	70'	28'	95'	1500W MZ	11	11	0			
2	B1, B5	90'	-	90'	1500W MZ	11	11	0			
2	B2, B4	90'	-	90'	1500W MZ	10	10	0			
1	B3	90'	-	90'	1500W MZ	8	8	0			
2	C1-C2	70'	-	70'	1500W MZ	8/6	14	0			
11	<b>TOTALS</b>								114	114	0

\* This structure utilizes a back-to-back mounting configuration



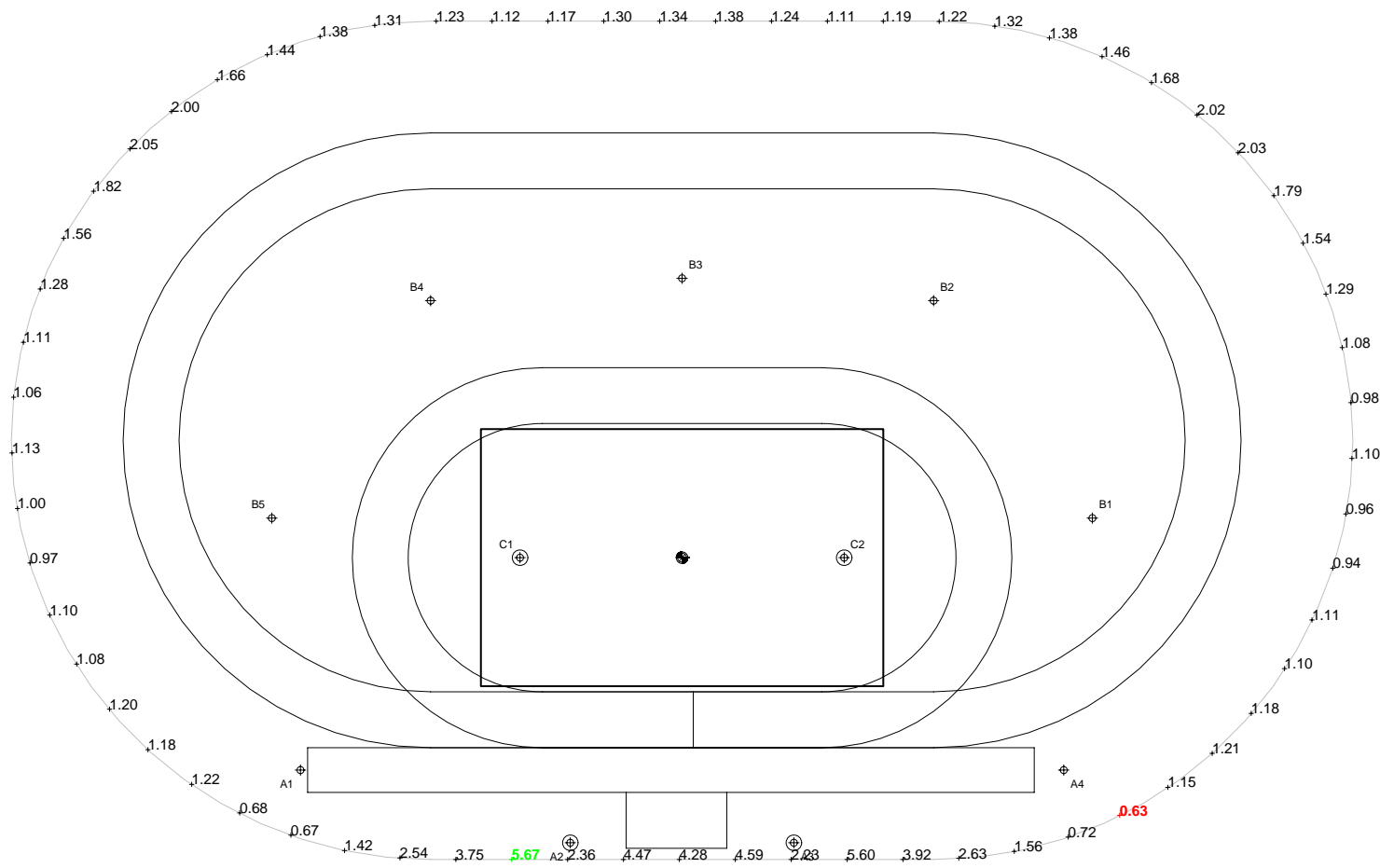
GUARANTEED PERFORMANCE

**ILLUMINATION SUMMARY**

100' Spill  
 Altamont Motorsports Park  
 Tracy, CA.  
 100' Spill  
 - Grid Spacing = 50.0'  
 - Values given at 3.0' above grade  
 - Luminaire Type: Green Generation  
 - Rated Lamp Life: 5000 hours  
 - Avg Lumens/Lamp: 134,000

CONSTANT ILLUMINATION HORIZONTAL FOOTCANDLES	
Entire Grid	
No. of Target Points:	66
Average:	1,690
Maximum:	5.67
Minimum:	0.63
Average Lamp Tilt Factor:	1.000
Number of Luminaires:	114
Avg KW over 5000 hours:	178.3
Max KW for Sizing Transformer:	193.8

**Guaranteed Performance:** The CONSTANT ILLUMINATION described above is guaranteed for the rated life of the lamp.  
**Field Measurements:** Averages shall be +/-10% in accordance with IESNA RP-6-01 and CIBSE LG4. Individual measurements may vary from computer predictions.  
**Electrical System Requirements:** Refer to Amperage Draw Chart and/or the "Musco Control System Summary" for electrical sizing.  
**Installation Requirements:** Results assume +/- 3% nominal voltage at line side of the ballast and structures located within 3 feet (1m) of design locations.





- Exhibit A: Current Aerial Site Photographs**  
**Exhibit B: 1996 CUP Site Plan**  
**Exhibit C: Existing Grading**  
**Exhibit D: Existing Drainage**  
**Exhibit E: Existing Lighting Plan, Sheets 1-6**  
**Exhibit F: Existing Pit/Paddock Lighting Plan**  
**Exhibit G: Regional Fault Map**

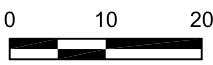
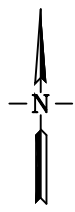
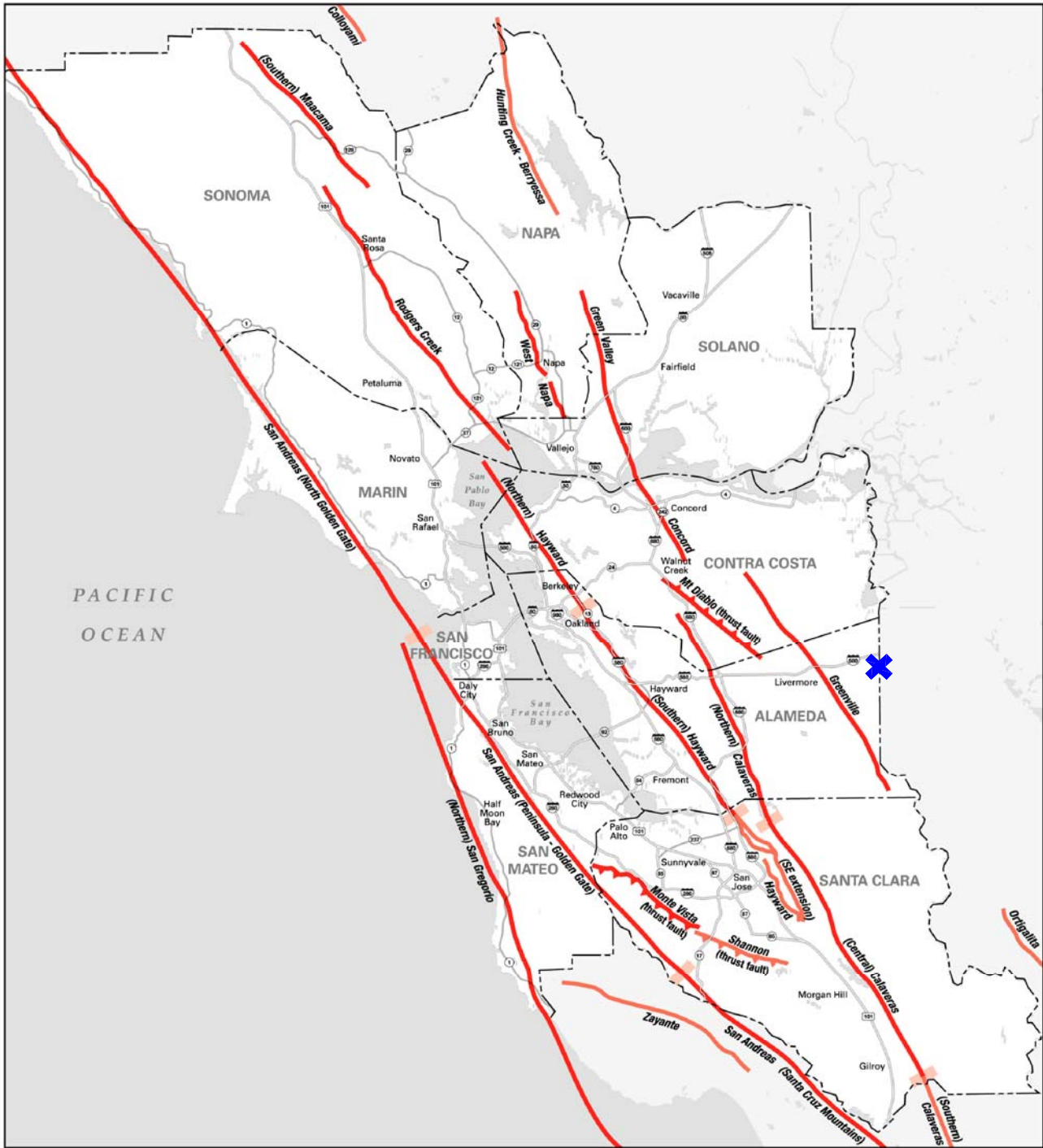
# Altamont Motorsports Park Pit/Paddock Lighting Plan



**● = Six (6) Existing 30' Light Poles & Fixtures to Remain**  
(each pole fitted with four-to-eight (4-8) 1,500W fixtures)



- Exhibit A: Current Aerial Site Photographs**  
**Exhibit B: 1996 CUP Site Plan**  
**Exhibit C: Existing Grading**  
**Exhibit D: Existing Drainage**  
**Exhibit E: Existing Lighting Plan, Sheets 1-6**  
**Exhibit F: Existing Pit/Paddock Lighting Plan**  
**Exhibit G: Regional Fault Map**



Scale (Miles)

**FIGURE 3**  
Regional Fault Map



Altamont Speedway  
Tracy, California

Project No. 1160.01